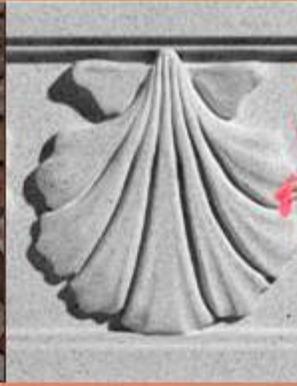


# PORT ST. JOE REDEVELOPMENT AREA DESIGN GUIDELINES



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## I. INTRODUCTION

The City of Port St. Joe (PSJ) established the Port St Joe Downtown Redevelopment Agency in December of 1989 and created the initial redevelopment plan in 1990. In 2005, the City updated the 1990 redevelopment plan to harmonize the shared community concerns and community development objectives as articulated by the 1990 Port St. Joe Downtown Redevelopment Plan, the 2006 Friends of North Port St. Joe Strategic Plan, and the Waterfront Committee Community Master Plan. The updated plan included a series of recommended activities intended to address the deterioration and underutilization of sites in the PSJ Redevelopment Area (PSJRA). These design guidelines address the architecture and site design issues that were identified in the community workshops and articulated in the updated redevelopment plan.

Port St. Joe Redevelopment Area/Conceptual Land Use Master Plan



To implement the PSJ Redevelopment Plan, the City has undertaken the establishment of design guidelines that will improve the image and appearance of commercial areas, as seen from public right-of-way. The design guidelines include standards for preferred development styles, materials, setbacks, landscaping and streetscape standards. The design guidelines introduce a consistent development theme that will ensure the design of new development to be visually sensitive to the existing development and the natural environment that makes Port St. Joe unique.

### A. PURPOSE

The purpose and intent of these design standards is to guide development toward creating a cohesive urban pattern of buildings and streetscapes that balances the pedestrian and traffic needs of residents, while it protects and enhances the original pattern of development. The basic premise is that quality appearance will result in quality development. Design standards will also protect the community's investment.

When the aesthetic appearance of a community is maintained or improved, a sense of pride develops for the residents, owners, and merchants.

The design standards are intended to be flexible and encourage design diversity and variation. Special attention has been placed on the creation of a quality, safe and functional urban environment. While the City encourages economic growth and commercial development in areas with access to arterial and collector roadways, it also encourages protection of residential and rural interests from the impacts of non-residential development. The integration of flowing traffic circulation and pedestrian accessibility is one primary concern for new development and redevelopment. The City encourages the design of new development and development to be visually sensitive to surrounding development and the environment.

## **B. APPLICABILITY**

The design principles contained in this document are applicable to all types of residential and non-residential development and redevelopment within the PSJRA. The fundamental design standards that are applicable to all non-residential development and redevelopment are contained in the Non-Residential General Design Standards section (Section III).

Because of the size of the PSJRA and the variety of land uses it contains, it was necessary to establish general standards that are applicable to entire PSJRA and also establish more specific standards based on the different types of corridors and their unique attributes. These corridor specific guidelines are intended to establish an architectural/streetscape theme that compliments the City's existing development, uses and heritage. The specific corridors addressed are:

- Downtown Streets & Martin Luther King Boulevard
- Major Highways (US 98 and State Highway 71/5th Street)
- 1st Street/Marina Drive/Baltzell Avenue



## II. THE PORT ST. JOE REDEVELOPMENT AREA (PSJRA)

The PSJRA has an area of approximately 428 acres, comprised of 915 parcels. The PSJRA is bounded by US 71 on the south; Royal Street on the east; Clifford Sims Drive, Avenue F and Avenue G on the north; and US 98 (from Avenue G to Marina Drive) and the Gulf of Mexico (south of Marina Drive) on the west.



### A. PSJRA EXISTING CONDITIONS

Avenue A, which runs east-west, divides the PSJRA into two parts with very different land uses in each. The area north of Avenue A is mainly dominated by single family residential development with some neighborhood level commercial uses along Martin Luther King Boulevard. The area south of Avenue A is mainly industrial and heavy commercial. The area south of 1st Street and west of US 98 is the downtown of Port St Joe, which consists of small retail establishments and restaurants. The streets in PSJRA are laid out in a regular grid form and lend themselves to the creation of a pedestrian-friendly atmosphere.

Florida Vernacular and Craftsman are the two predominant architectural styles in the PSJRA. Buildings are one to two stories high with some new residential construction on the east side. Apart from the newly developed areas, many of buildings located in the north PSJRA are in a deteriorated condition.

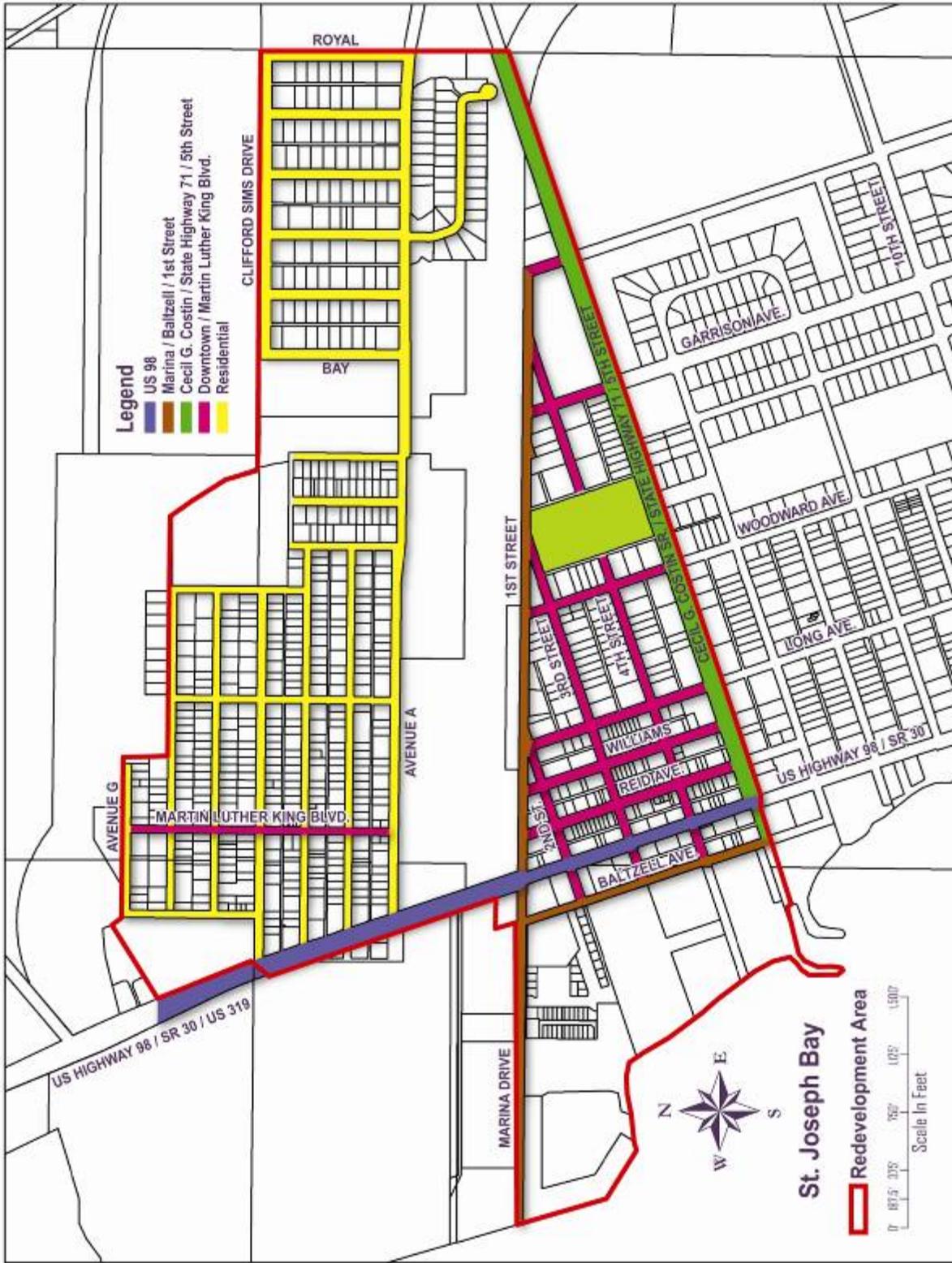
### B. DESIRED IMAGE

To continue and strengthen the urban development characteristic of the area and to maintain the existing pattern of development, general design standards were created for the entire PSJRA. Additionally, due to the mixture of land use and their distribution in the PSJRA, more specific guidelines were created for the following sub-areas:

- Residential Districts
- PSJRA Downtown Area (Downtown Streets)
- US 98
- Marina Drive (1st Street) and Baltzell Avenue
- State Road 71 (5th Street)



KEY MAP



PSJRA General Design Guidelines



### III. RESIDENTIAL NEIGHBORHOODS

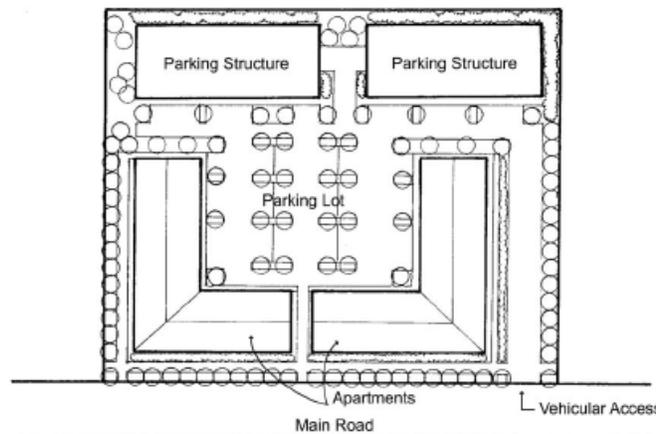
The standards contained in this section apply to new residential development and redevelopment outside the sub-areas noted in other sections of this report. The standards address architecture, site layout, and lighting. There are no requirements for the provision of streetscape furnishings.

#### A. RESIDENTIAL ARCHITECTURE

All new residential development and redevelopment shall adopt one of the two predominant architectural styles: Florida Vernacular or Craftsman. Appendix A contains a description of each style.

#### B. SITE LAYOUT

- Single-family developments should avoid locating garage doors that project forward from the front facade of the residence. Garages are encouraged to be located to the side or rear of a residential lot. Shared driveways are encouraged between residences to increase the amount of open space per lot.
- The existing grid street pattern shall be maintained. Street closures and cul-de-sac streets shall be prohibited.
- Parking for multi-family developments should be located behind the front building façade. Garages or carports in multifamily developments may be grouped but shall also be located behind the residential buildings, away from public view.



- Trailers and boat parking shall be located behind the buildings and shall be fully screened from the view of adjacent properties and roadways.

#### C. LIGHTING

Lighting is important to ensure safety for both pedestrians and vehicles. The following standards are applicable to the residential areas in the PSJRA.

- Exterior lighting shall be designed and installed so that it illuminates without creating glare or light spillage; safe for automobile and pedestrian traffic; and without distorting natural color.
- No light fixtures shall be placed where they would conflict with the location of existing or proposed canopy trees.
- Electric wiring shall be provided underground.



- Lighting should conform to “dark sky” standards including downward projection, “full cut-off” fixtures that shield light from being emitted upward toward the sky or surrounding areas.
- Residential Developments that include lighting fixtures shall use the following specifications:
  - Light poles shall not exceed thirty (30) feet in height.
  - The required illumination for vehicles in residential neighborhoods shall be 0.60 footcandles maximum.

**Required Model for Streetlights in Residential Areas:**

Sternberg (1)1912 G3-CAS-6-OK-30-E-OFC-175  
PMH-MT-BK



**D. UTILITIES**

Utility lines for all new residential developments and redevelopment are required to be located underground. Utility boxes must be totally screened from view of right-of-way.



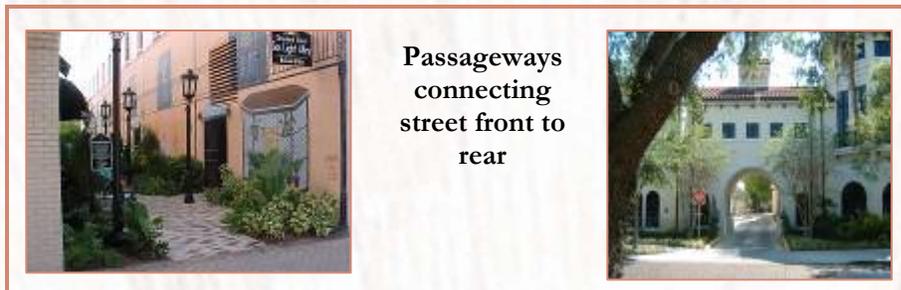
#### IV. NON-RESIDENTIAL AREAS - GENERAL STANDARDS

The following standards apply to non-residential development in the PSJRA. The standards address the relationship between building facades and the public realm, the form and mass of buildings in relation to one another and the street, and the scale and types of streets and blocks.

##### A. BLOCK LAYOUT

The configuration of blocks and lots is very critical for corridors that are designed predominantly for foot traffic rather than vehicular traffic. All the areas in the PSJRA, except for the US 98 and SR 71 corridors (which carry high volumes of traffic), are predominantly pedestrian oriented. To maintain and enhance the pedestrian-friendly urban character of the PSJRA, as well as to achieve a balance between the automobile and the pedestrian activities, it is important to preserve the current size of the blocks, rather than allowing the consolidation of smaller blocks into mega blocks.

- Large-scale developments located anywhere in the PSJRA should provide pedestrian passageways/courtyards connecting the street front to the rear parking areas and alleys, to maintain a pedestrian’s ability to migrate through the commercial area (see examples below).



**Passageways connecting street front to rear**

- New development should not be allowed to vacate existing rights-of-way, including alleys, to form mega/super blocks.
- Redevelopment of properties/blocks where alleys have been vacated should be required to establish new alleys consistent with the existing system and the standards of **Section IV.F.3.**

##### B. BUILDING PLACEMENT AND ORIENTATION

###### 1. Building Placement

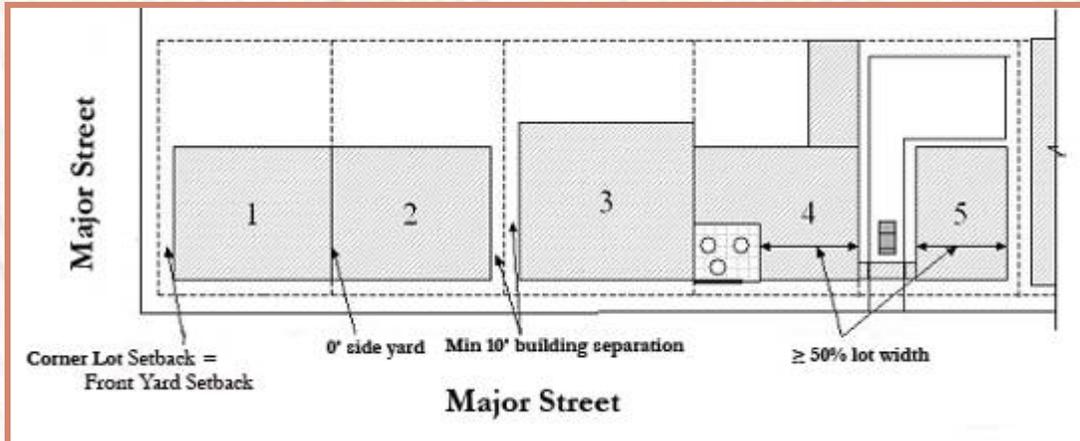
All new development in the PSJRA shall adhere to the following setback requirements, unless otherwise noted in the specific areas later in this report:

###### a. Front and Corner Setbacks:

Buildings should be located as close to the street as possible. However, it is important that sufficient width is preserved along the sidewalks to provide for adequate walking space and room for formal streetscape elements (street trees, lighting, trash receptacles, benches, etc.).



- The minimum front and corner setbacks shall be the edge of the sidewalk.
- The maximum front and corner setbacks shall be five (5) feet from the edge of the sidewalk.
- The maximum front and corner setbacks may be permitted to extend up to twenty (20) feet (for no more than 50% of the building frontage) if pedestrian courtyards, plazas, cafes, fountains or other public gathering places are provided in front of the recessed portion of the building.



**b. Side Yard Setbacks:**

Side yard setbacks should be determined based on the existing development on the adjacent lots as follows:

- The minimum side yard setback is zero (0) feet and the maximum side yard setback (both side combined) shall not be greater than fifty (50) percent of the lot frontage.
- If the adjacent development was built up to the side property line, then the development on the subject site should be built up to the property line.
- If the adjacent development has a side yard setback of less than ten (10) feet, then the proposed development will need to allow for a ten (10) foot minimum separation between buildings. The gap between the buildings shall be designed per **Section IV.C.3**

**c. Rear Yard Setback:**

Rear yard setbacks shall be per City Code, except that it is important to allow for a rear yard setback large enough to preserve the continuity of parking areas in the rear.

**2. Building Orientation**

Building orientation refers to the location of the primary, secondary and rear facades and entrances to a building. The placement and orientation of a building on a site has a major impact on the appearance of the built environment. The orientation of the front façade is important for the pedestrian viability of an area.

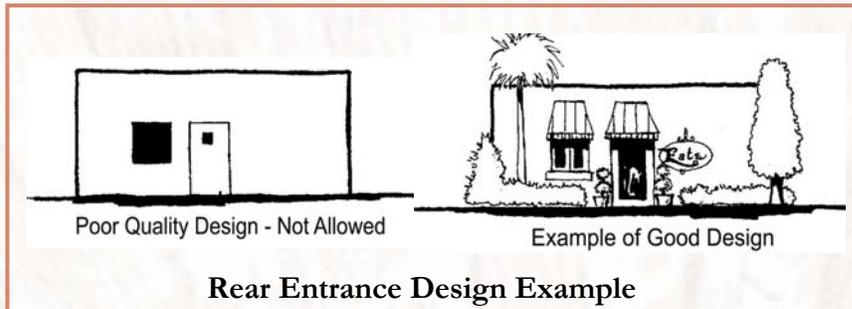


Buildings that turn perpendicular to the public right-of-way or only have access from rear parking lots create an environment that is unfriendly for pedestrians.

- Building shall be located close to the street and shall be placed parallel to the right-of-way they front. Parking and stormwater shall be located in the rear of the site. In the event that parking or stormwater extend to the side of the building, it shall be screened from public view by a street wall (See **Section IV.G.4**).
- Buildings should orient the primary façade to the public right-of-way. If the building fronts on more than one public-right-of-way, all the facades facing the public right-of-way shall be designed consistent with the front façade.



- When parking is provided behind the business or the rear façade faces an alley, a **secondary** entrance may be provided in the back of the building.



- Building setbacks shall be as specified for each sub-area in this report. Any areas not addressed by the sub-area standards shall comply with the setbacks noted in the City’s land development regulations.

**C. BUILDING FORM AND ARCHITECTURE**

Buildings are the most important component of the urban streetscape form. They set the mass, scale and tone of the built environment. As such, all of the various elements of a building’s architectural makeup are important in putting together a complete composition. Elements such as materials, colors, surface details, texture, doors, windows, and roof shape contribute to a well-designed urban fabric. The following section provides the architectural styles and design regulations that are applicable to all new non-residential development and redevelopment in the PSJRA.



**1. Preferred Architectural Styles**

New buildings should complement the historic and architectural heritage of the area.

- New buildings should adopt one of the two architectural styles typical of the area: Florida Vernacular and Craftsman (see photos below). Please refer to **Appendix A** for detailed descriptions of the styles.
- All the elements that comprise a building, including the building form, should be consistent with the style selected.
- If an existing structure that possesses a particular architectural style is proposed for redevelopment or expansion, then the following guidelines shall be followed:
  - Doors, and door details, windows, frames, lintels, fan lights, sidelights, pediments and transoms, in good condition or repairable that are in character with the style and period of the building should be retained.
  - If doors, windows, or door/window details on principal facades are found to be irreparable, they should be replaced with new elements that match the original in material, size, orientation and configuration.
  - Additions to existing buildings shall match the original windows and doors in size, material, orientation and configuration as in the principle structures.



**2. Façade Design**

Buildings shall have architectural features and patterns that provide visual interest from the perspective of the pedestrians and motorists. Buildings shall include substantial variation in massing such as changes in height and horizontal planes. All additions and alterations shall be compatible with the principal structure in design, color and materials. The following are general façade standards applicable to all non-residential development in the PSJRA:

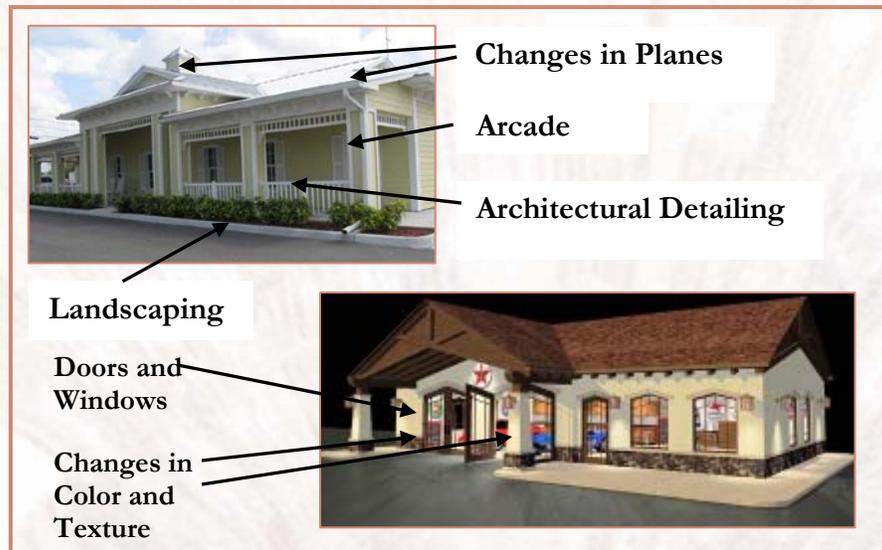
**a. Façade Variation**

The design of the primary facade of non-residential buildings is critical for the atmosphere that will be created along the street front. Primary building facades shall be enhanced by the use of vertical and horizontal



elements. The design of secondary and rear facades shall be consistent with the front façade.

- Major architectural treatments, such as cornices, arches, exposed brackets, overhangs, and repeating details, should be continued around all sides of the building that are visible from the public realm.
- Horizontal masses should not exceed a height to width ratio of 1:3 without providing a substantial architectural element that either projects up and out, such as a tower or bay, or an architecturally prominent public entrance. The architectural element shall have a difference in plan of no less than two (2) feet.
- Primary facades shall not exceed twenty (20) horizontal feet, or ten (10) vertical feet, without three (3) of the following elements. Secondary facades shall include at least two elements. When selecting these elements, there shall be a combination of vertical and horizontal elements, so that there is variation.
  - A change in plane, such as an offset, reveal or projecting rib (columns, planters, arches, voids, etc.). Such plane projections or recesses shall have a width of no less than twenty (20) inches, and a depth of at least six (6) inches.
  - Architectural details such as raised bands and cornices.
  - Awnings and/or arcades.
  - Complementary change in material/texture and color.
  - Doors and/or windows.
- The ground floor of the primary façade shall have at least two (2) of the following elements for at least sixty (60) percent of the width of the façade:
  - Public entrances
  - Windows/display windows
  - Arcades, awnings or other treatment to provide shade to pedestrians





**b. Corner Facades**

Buildings on corner lots are considered to have double front façades for architectural review purposes. Corner lots at the intersection of arterials and/or collectors shall be designed with architectural embellishments to emphasize their location whether they function as gateways or major community transition points.

- Buildings on corner lots shall include at least one of the following embellishments: cornice detail, arches, peaked roof forms, corner towers, clocks, bells and other design features.
- Hardscape design, such as pedestrian plazas with artwork or fountains, may substitute for building embellishments on corner lots.

<b>Corner Buildings</b>	
<b>Appropriate Examples</b>	<b>Inappropriate Examples</b>
	
	

**3. Façade Continuity**

Façades along public streets should limit the number of gaps along the block. If a gap is created between two buildings, a pedestrian courtyard (connecting to rear parking areas or alleys) or a decorative façade connecting the two buildings should be created within that gap (see examples below).

Alternatively, a low street wall may be provided along that portion of the lot along the right-of-way not devoted to pedestrian or vehicular access.



#### 4. Entrances

The following are design standards for entrances to multi-family and non-residential developments:

- All primary entrances shall have a protruding front gable, pilasters, columns, a stoop or other projection or recession in the building footprint, a minimum of three (3) feet in depth that clearly identifies the entrance.



**Well-Defined Entrances**

- In addition, every primary entrance shall have two (2) other distinguishing features from the list below:
  - Variation in building height;
  - Canopy or portico;
  - Raised cornice or parapet over door;



- Arches/columns;
- Ornamental and structural architectural details other than cornices over or on the sides of the door;
- Covered drop-off areas are recommended, especially to multi-family and institutional buildings.
- The use of porches is encouraged. They may have balconies or verandas above them, which may encroach into the setback up to five (5) feet. Porches should be open and non-air conditioned, except that up to 25% of the porch may be screened if not encroaching into the right-of-way.
- Protection from the sun and adverse weather conditions for patrons should be considered for the entranceways. Protection methods may include awnings, canopies, colonnades, porticoes or similar treatments meeting the intent of this section.

## 5. Fenestration

Depending on the location of the subject building, the arrangement of windows and doors should be consistent with the neighboring buildings. For new construction, all windows should be consistent with the style of the structure.

Glass shall be transparent, without color, except for stained or art glass, provided the stained or art glass is in character with the style of the building (churches, craftsman buildings, etc.).

The transparency/translucency requirement shall apply only to new construction or to those areas of a structure that are altered as part of a substantial enlargement or improvement. Transparency requirements for both residential and non-residential uses include the following:

- In commercial areas, the ground floor building wall facing the street shall contain windows and doors occupying at least fifty (50) percent of the first floor façade. The windows shall be located between three (3) and seven (7) feet measured from ground level.
- Windows in commercial areas shall comprise at least twenty-five (25) percent of the overall street side exterior elevation.
- For commercial buildings, clear glass (88% light transmission) should be installed on the first floor. Tinted glass allowing a minimum of fifty (50) percent light transmission should be considered only for use on second floor windows and above.
- Residential and office uses shall have front exterior walls containing a minimum of twenty-five (25) percent transparent or translucent materials on each story below the roof line. The side exterior walls (facing the street) shall each contain a minimum of fifteen (15) percent transparent or translucent materials on each story below the roof line.
- Windows and doors should be glazed in clear glass with no more than ten (10) percent daylight reduction. The use of reflective glass and reflective film is prohibited on all buildings. Glass block is not considered transparent and is not permitted in storefront windows.



- Transparent materials on walls that are not parallel or approximately parallel to the public right-of-way and on doors shall not be counted toward the minimum transparency requirement.
- Garage doors shall not be included in the transparency/translucency calculation.

**Transparency Examples**

<b>Appropriate</b>			
<b>Inappropriate</b>			
<b>Not Enough Transparency</b>	<b>Too Much Transparency</b>	<b>Not allowed</b>	

**6. Building Materials**

Building materials can have a major impact on an area. Materials should not be selected on the basis of cost alone, but rather on compatibility with the building style and neighborhood character.

**Building Materials**

		
<b>Appropriate</b>		<b>Inappropriate</b>

- Materials for new construction should be selected from among those that are typical of the architectural style selected.
- Building materials should reflect quality and stability in terms of durability, finish and appearance.
- Cedar shakes, unfinished block, corrugated or reflective metal panels, textured plywood, and plastic siding shall not be used.
- Mirrored glass and glass walls are prohibited.
- Exterior building materials that are encouraged include wood siding, shingles, wooden clapboard, brick or materials having appearance of brick, stone,



wood. Horizontally struck stucco, exterior insulated finish system stucco panels, board and batten, wood or vinyl siding, and stained hardwood panels shall also be considered acceptable finishes.

- The scale of the building materials used should respond to the scale of the proposed building.
- The exterior facade material for an addition or renovation shall either be the same as the existing building, a material that simulates the original material, or a combination of the two.
- The City may approve an alternative material for building additions if it is shown to be compatible with the materials used on the existing structure.

**7. Building Colors**

Colors have a strong visual effect when it comes to building design. Building colors should be selected based on the architecture of the building (each style has a range of characteristic colors) and trying to achieve harmony with adjacent buildings. Fluorescent and intense colors do not harmonize but create stark contrasts and can be overpowering. The following are the guidelines that need to be followed in the area to achieve harmony, without restricting creativity and individuality.

- Building colors shall be selected according to the building style. For instance, craftsman style buildings should use earth colors (white, beige, terracotta, or tan), while Florida vernacular should use pastel shades or white.

**Styles and Colors**



**Florida Vernacular**



**Craftsman**

- No more than three (3) different colors or color shades should typically be used on a single building (residential and non-residential).



- The use of intense, florescent or day glow colors, or colors that are garish, gaudy, loud, excessive, and ostentatious or otherwise constitute a glaring and unattractive contrast to surrounding buildings shall be prohibited.



- Black as the predominant exterior building color and monochromatic (all one-paint color) schemes are prohibited.
- All colors used on a building must be complementary and should not be used to advertise the business. As an example, yellow and red are not complimentary colors and are typically chosen by merchants that want to increase advertising and stand out drastically from other merchants. Clashing trim colors will not be permitted.
- A solid color band or color stripes shall not be used for architectural detail, unless the band is consistent with the style and the color of the building.



### 8. Roof Design

The style of the roof must be consistent with the style of the building and shall adhere to the following requirements:

- Buildings shall have a recognizable top consisting of, but not limited to:
  - Cornice treatments;
  - Roof overhangs (with brackets if consistent with style);
  - Stepped parapets;
- The roof design for an addition shall either be the same as the existing building or shall be a combination that includes the existing design. The City

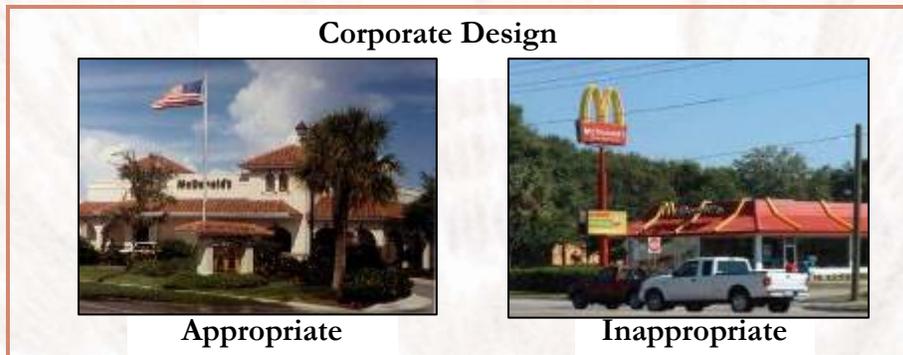


may approve an alternative roof style for an expansion, if the new roof is constructed in the same building style as the existing roof.

- Roof forms shall relate to those found in the area. For the portion of a building visible from the pedestrian level, repetition of the existing or traditional roof shapes, pitches, and materials on new construction shall be considered one way of making new structures more visually compatible.
- Mansard roofs shall be prohibited. Flat roofs shall be hidden from public view by a parapet of no less than three (3) feet in height.
- Cornices are encouraged. They may be simple or mixed (straight and curved moldings), but should not exceed twenty-four (24) inches in width. Cornices shall have a minimum of twelve (12) inches in height, and a minimum of three (3) vertical (not diagonal) changes in plane, and a variety of thickness in relief ranging from the greatest at the top to the least at the bottom
- Roofs shall have no less than two (2) of the following features:
  - Parapets not exceeding fifteen (15) percent of the height of the supporting wall. The highest point of the parapet shall not at any point exceed thirty (30) percent of the height of the supporting wall.
  - A three-dimensional cornice treatment.
  - Overhanging eaves, extending no less than three (3) feet past the supporting walls.
  - Sloping roofs that do not exceed the average height of the supporting walls, with an average slope greater than or equal to one (1) foot of vertical rise for every four (4) feet of horizontal run and less than or equal to one (1) foot of vertical rise for every one (1) foot of horizontal run.
  - Three (3) or more roof planes per primary façade.
- Acceptable materials for sloping roofs include standing seam, terracotta tiles and architectural asphalt shingles, depending on the building style.

**D. SPECIFIC USES**

**1. Corporate Franchises**



Corporate franchises should not be allowed to create visual clutter and to use architecture and building colors to act as signage. Therefore, exceptions to these guidelines shall not be made for corporate franchises. National corporate chains



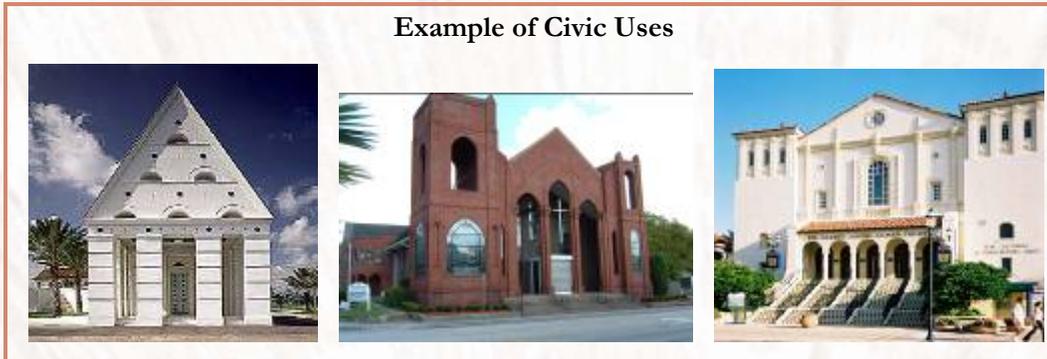
that typically design their buildings to read as signage have been known to modify their designs to blend with the character of the neighborhood. Examples of appropriate and inappropriate corporate structures are shown below. These examples shall be used as models for future corporate design and development within the PSJRA district.

**2. Civic and Institutional Buildings**

Many communities are recognizable by their public squares, plazas and civic structures. Civic structures often become landmarks and public gathering places that participate in establishing an identity for the community through distinguishing architectural design.

Civic buildings contain uses of special public importance, such as municipal services, churches, libraries, schools, recreation facilities, and places of assembly. Civic uses should be placed in central locations as highly visible focal points. Where feasible, they should be close to transit stops for ease of use by pedestrians. Civic structures should not be located within storefronts, shopping centers or malls and should not blend with adjacent developments, but rather they should stand out. They are monuments to be recognized as landmarks and appreciated by residents and visitors alike. These types of structures are intended to break the continuity of the streetscape. However, the design, materials and colors should be tasteful and not garish.

**Example of Civic Uses**



**3. Drive-Through Establishments**

Drive-through facilities shall be discouraged in PSJRA, because of the potential conflicts with pedestrian traffic and the interruption of façade continuity necessary for vehicular access. The following standards shall apply if drive-through facilities are permitted in the PSJRA:

- Only one drive-through window per business, with stacking for 3 cars, shall be allowed in the PSJRA.
- Drive-through windows shall be oriented to the side or the rear of the site so that they are not directly visible from the public right-of-way.
- Drive-through lanes must be designed with pedestrian safety as the first priority.

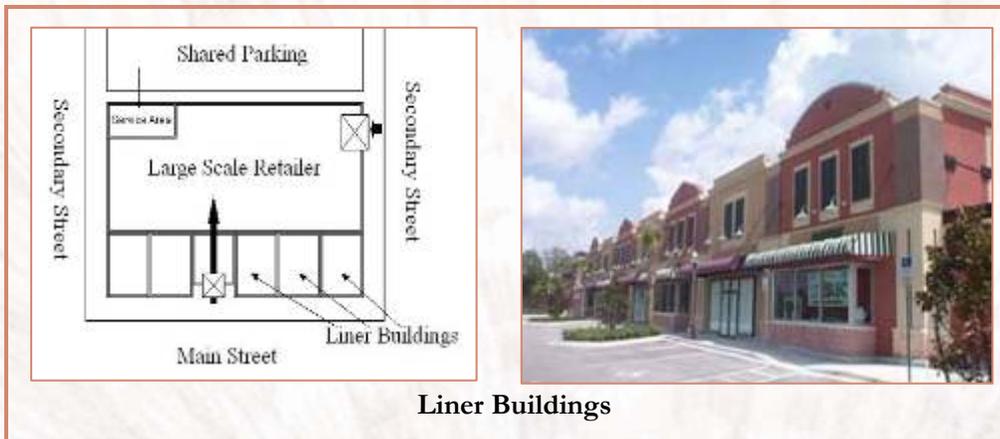


- Drive-through structures must have the same detail, materials and roof design of the principal structure.
- A dense hedge of evergreen shrubs shall be provided for the entire length of the drive-through cueing or stacking area in the following manner to screen drive-through windows from the public right-of-way:
  - At initial planting and installation, shrubs shall be at least thirty (30) inches in height and shall be planted thirty-six (36) inches or less on center.
  - Within one (1) year of initial planting and installation, shrubs shall have attained, and be maintained at, a minimum height of four (4) feet and shall provide an opaque vegetative screen between the street and the drive-through.
- A pass-through lane shall be required for all drive-through facilities. The pass-through lane shall be constructed adjacent to the stacking lane(s) in order to provide a way out of (or around) the stacking lane(s).

**4. Big Box Development**

Anchor stores that exceed 20,000 square feet have been referred to in the past as “big box” developments. The nickname resulted from many of the buildings designing their stores in the shape of a box with no architectural style and very little detail.

To prevent this box appearance, liner buildings shall be provided along the first floor facing the public right-of-way. The liner building would either have actual smaller shops located facing the street or the appearance of smaller shops with window-shopping opportunities. The liner buildings provide a transition from the street to the larger anchor store.



**Liner Buildings**

**5. Outparcel Development**

Frequently, large shopping centers have a few merchants that develop stand alone buildings, known as outparcels, at the perimeter of the site adjacent to the



public right-of-way. Large-scale developments that have a primary building and secondary outparcels must conform to the following standards:

- In order to enhance the visual impact of outparcels, all exterior façades of the outparcel buildings shall be considered primary façades and shall employ architectural embellishment and landscape design treatments on all sides.
- Pedestrian connections between the main structure, adjacent outparcels, and the public sidewalk are required.
- Vehicular connections between outparcels, the main structure, and adjacent outparcels shall provide for safe and convenient movement within the site.
- Consolidated parking is encouraged to reduce the amount of asphalt.

**E. SITE CIRCULATION ACCESS AND PARKING**

The following standards are intended to guide the design of access, parking and circulation system to achieve both safety and functionality, and to enhance vehicular connectivity throughout the district.

**1. Pedestrian Access and Circulation**

- The City of Port St Joe supports the use of alternative modes of transportation to provide access for all residents, including the elderly, youth and physically impaired. Safe, comfortable and consistent pedestrian connections are required.
- Sidewalks are required on both sides of the streets.
- All major intersections should have crosswalks.
- Sidewalks/walkways shall be provided to connect the primary building to public streets, external sidewalks, and outparcels.
- Pedestrian walkways within the development shall be differentiated from driving surface through a change in material.
- Sidewalk within private developments shall be a minimum of five (5) feet wide.
- To increase pedestrian safety, sidewalks and walkways shall be slightly raised at a different grade than the vehicular parking lot, and shall be landscaped and have pedestrian scale lighting.
- Whenever feasible, walkways should be shaded by canopy trees, arbors, awnings or a combination of these.



**Pedestrian Walkways**



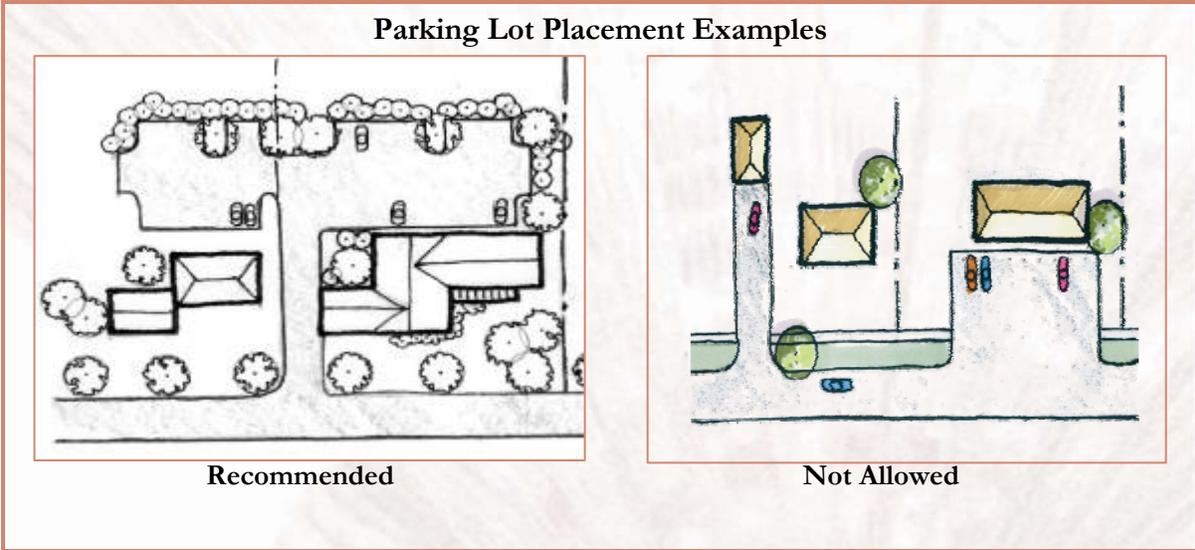
**2. Vehicular Access, Circulation and Surface Parking**

The following standards are intended to avoid large expanses of asphalt and increase the coordination between property owners to achieve a continuous and safe environment for both automobile and pedestrians.

- Not more than ten (10) continuous parking spaces may be allowed without a landscape island of 200 square feet minimum.
- Each landscaping island shall have one (1) acceptable canopy tree or three (3) understory trees, five (5) shrubs and grass cover.
- As an incentive to reduce asphalt and encourage increased landscaping, parking space depth may be reduced by two (2) feet if abutting a landscape area and the vehicle will not interfere with required planting or encroaching on a sidewalk. Continuous curbing or wheel stops shall be used at the end of the eighteen (18) foot dimension.



- Parking should be located behind the front building façade to prevent it from dominating the image of the site. Where this is not possible, a streetwall will be required to screen the parking area.





- The use of joint access easements is required between sites to reduce the number of access points and driveway area, and to increase the amount of landscaping.
- Access to off-street parking shall be by a private or shared drive. Pull-off parking directly off of the road is prohibited.
- Access driveway width should be limited to fifteen (15) feet for one-way driveways and twenty-four (24) feet for two-way driveways. Should a vehicular access from the main road be necessary due to the lack of access to rear parking from a side street, an alley, or an adjacent property (shared use agreement necessary), the driveway width in this case should not exceed twelve (12) feet and a gateway, arch or similar feature should be provided to preserve the block continuity.
- When it is feasible to provide vehicular access to a commercial site from a secondary street, without negatively impacting single-family residential uses, it shall be required.
- Trailers and boat parking shall be located behind the buildings and shall be fully screened from the view of adjacent properties and roadways.



**3. Parking Structures**

Currently, there are no public or private parking structures in the PSJRA area. However, parking garages are an integral part of an urban area, and sooner or later the City may find the need to provide them in this district. Because of their function, structured parking facilities are usually designed as large uninteresting boxes, with very little attention to the pedestrian scale. The design of a public garage can have a major impact on the appearance of an area. The following guidelines will assist with the creation of attractive and compatible garage structures in the PSJRA district. The following guidelines are applicable to all structured parking lots in the district.

**a. Parking Structure Placement**

Parking structures should not occupy lots with frontage on main streets, but should be located behind the commercial strip. The location should enable the placement of liner commercial buildings between the parking structure and a sidewalk on the main street.

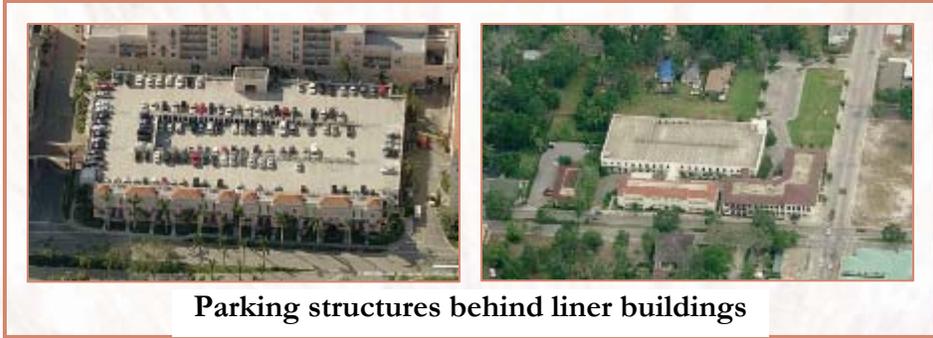
**b. Parking Structure Facade Design**

- Along the streets in the downtown, US 98, SR 71, 1<sup>st</sup> Street (Marina Drive) and Baltzell Drive, at least seventy-five percent (75) of ground floor parking garage frontage, exclusive of entrance driveways, stairwells and pedestrian entryways, shall consist of active uses, such as offices, light retailing, personal services, and entertainment. This requirement is intended to



create pleasing pedestrian-oriented spaces. The City may approve other design alternatives, such as a liner building around the outer edge of the garage, designed to achieve this intent.

- The entrances to structured parking garages should be from side streets and/or alleys.
- Those areas not including ground floor active uses shall provide a pedestrian-oriented landscaped setback of ten (10) feet maximum.
- The exterior facades of all parking garages fronting on pedestrian-oriented streets shall be designed to achieve an architectural unity with the block.



**F. PUBLIC REALM**  
**1. Right-of-way Design**

The following street sections shall be used for the construction of new two-lane roadways or major redevelopment within the PSJRA. If the new street construction is a continuation of an existing street the proposed right-of-way width may vary depending on existing conditions. The design of existing streets within specific sub-areas shall follow the standards noted in those sections. The



following standards apply to all streets, **unless noted otherwise in the sub-area sections:**

- Sidewalk shall be constructed of broom-finished concrete and shall be designed for heavy pedestrian traffic.

**Required Model for Sidewalks:**

Flagstone Pavers: Freedom  
 4"x8"  
 2-3/8 inches (60 mm)  
 Red, tan, charcoal blend



- Crosswalks are required whenever a pedestrian walkway intersects a vehicular area. They shall be six-feet (6) wide and shall be designed for heavy vehicular traffic. There shall be a header course along the edge of the crosswalk. The infill pattern shall be running bond.

**Required Model for Crosswalks:**

Flagstone Pavers: Freedom  
 4"x8"  
 2-3/8 inches (60 mm)  
 Red



- Two-lane local roads shall have on-street parking (parallel or angled depending on the available right-of-way width and existing conditions), which accomplishes three purposes:
  - Vehicular traffic is slowed down so that it is safer for pedestrians and parking maneuvers.
  - Convenient parking close to buildings.
  - A better design alternative to the typical surface parking lots that can be unsightly from the streets.

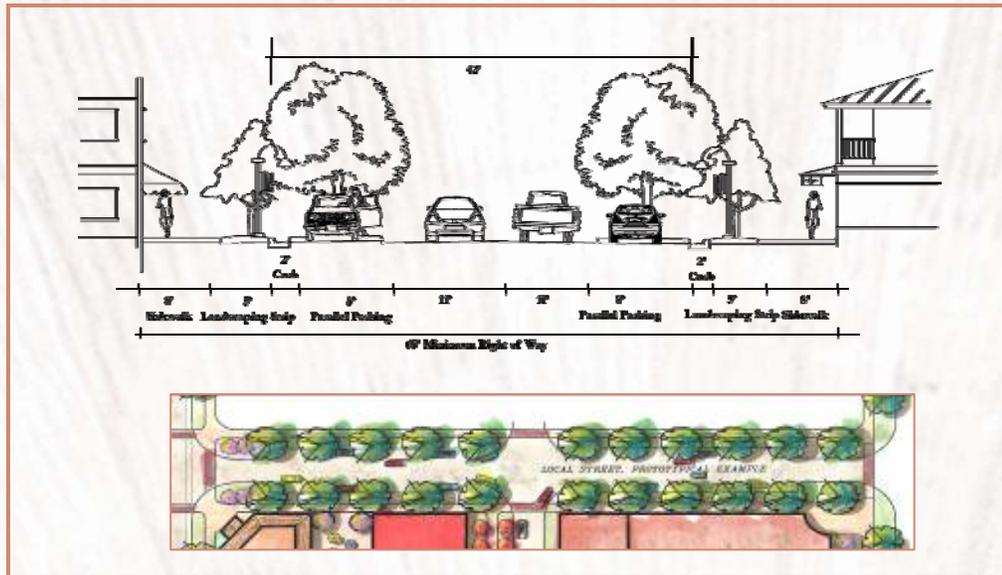
**a. Two-Lane Streets with On-Street Parallel Parking**

The two-lane street cross section with parallel parking should include:

- **Right-of-Way Width:** Sixty-eight (68) feet. If enough right-of-way is not available, the two (2) feet gutter width may be included in the parking space width.
- **Travel Lanes:** Two (2) eleven (11) foot wide lanes.
- **On-Street Parking:** Eight (8) foot wide by twenty-two (22) feet long parallel spaces on both sides of the road. Landscape islands ("bump outs") as wide as the parking area will be required at the end of each parking row.
- **Curb and Gutter:** Two (2) feet wide, Type "F".
- **Landscape Strip:** Five (5) feet wide between the parking row and the sidewalk. The landscape strip shall include:



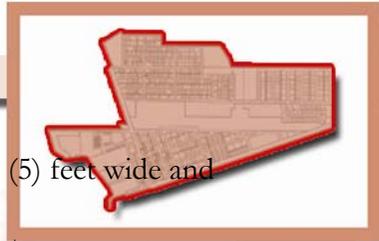
- Grass or other ground cover, except for one hundred (100) square feet of mulch around each shade tree.
  - Four (4) feet wide paved (broom finished concrete) pedestrian openings through the landscape strips approximately every forty (40) feet.
  - A canopy tree with a four (4) inch minimum caliper at planting planted every fifty (50) feet on center in the landscape strip.
- **Sidewalk:** Eight (8) foot wide sidewalk constructed of broom-finished concrete. Sidewalks are required on both sides of the street.



**b. Two-Lane Streets with On-Street Angled Parking**

The two-lane street cross section with angled parking should include:

- **Right-of-Way:** Ninety-six (96) feet wide.
- **Travel Lanes:** Two (2) eleven (11) foot wide lanes.
- **On-Street Parking:** Ten (10) feet wide by nineteen (19) feet long angled (60 degrees) spaces on both sides of the road. Wheel stops shall be used at the end of the nineteen (19) foot dimension to prevent cars from hitting the brick planters. Landscape islands ("bump outs"), as wide as the parking area, will be required at the end of each parking row. One canopy tree shall be planted in each landscape island.
- **Curb and Gutter:** Two- (2) feet wide, Type "F".
- **Warning Strip:** A one- (1) foot wide warning strip shall be installed between the curb and the 2-foot concrete surface required around the planter (see below). This header course of one and a half bricks shall match the brick of the planter.
- **Planters:** Raised brick planters with limestone caps (real or cast-stone) shall be placed between the parking area and the sidewalk.



- The planters shall be two (2) feet high, five (5) feet wide and twenty-four (24) feet long.
  - The planters shall be spaced twenty (20) feet.
  - There shall be a two (2) foot wide section of broom finished concrete on all four sides of the raised brick planters.
  - Two understory trees with a four (4) inch minimum caliper shall be planted within each brick planter.
  - The planters shall be completely landscaped with shrubs, groundcovers and/or seasonal color (annuals and perennials). See approved plant list (**Appendix B**).
- **Sidewalk:** Eight (8) feet wide (measured from the planter and including the 2-foot concrete section adjacent to the planter).



## 2. Pedestrian Passages

Pedestrian passages connecting rear parking lots and public sidewalks shall feature a pedestrian amenity. This may include public art, decorative fountains and/or shade structures such as a gazebo or trellises, an outdoor seating area, play area for children, or a courtyard.

- At least one significant pedestrian amenity, such as an outdoor seating area, play area for children, or courtyard, shall be provided for every 75,000 gross square feet of development.
- Required pedestrian amenities may be combined into one or more locations, in order to create a larger amenity



**Pedestrian Plaza**

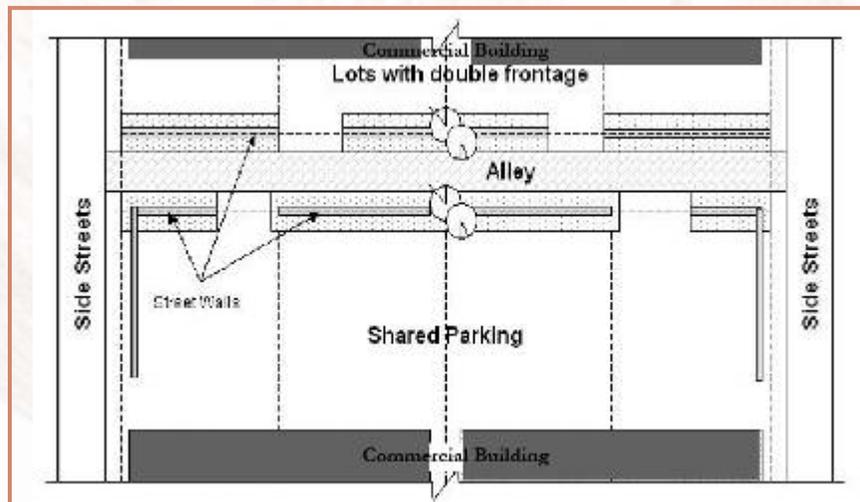


**Corner Pocket Park**

### 3. Alleys

Alleys contribute to making the primary storefronts and streetscape a more pleasing environment by moving the necessary service and delivery areas to the rear of a site. A system of alleys (approximately 12' wide) is already in place in the PSJRA. However, alleys have problems with appearance and maintenance. As the properties along the alleys develop and redevelop, the following standards shall be considered:

- A streetwall shall be provided between the alley and the parking area to delineate the alleyway area.
- A three (3) to five (5) foot wide landscape strip shall be provided on the inside of the wall.
- Alleyways shall be used to access parking lots.
- Service vehicles shall use the alleys to access sites.





**4. Bus Stops**

Large-scale retail development with one or more retail tenants of at least 50,000 gross square feet or more and adjacent to an arterial street, which is or may be used as a transit route, should provide on-site accommodations for public transit access, potentially including a bus pullout and shelter.

**5. Site Furnishings**

**a. Waste Receptacles**

Waste receptacles shall be installed on all streets. The waste receptacles shall be grouped with one (1) or two (2) plant containers. There shall be two (2) waste receptacles at each street intersection on opposite corners. Additional waste receptacles shall be spaced every nine-hundred (900) feet or three (3) per street forming a city block, which ever is greater.

**Required Model for Waste Receptacles:**

Victor Stanley Model VS-SD-42  
32 gallons  
Black



**b. Plant Containers**

Craftsman style planters in two different sizes shall be installed on all streets in conjunction with a waste receptacle. The planters shall be located between the sidewalk and the travel lane so that they shall not interfere with pedestrian traffic. Each plant container shall be planted with a feature plant of colorful foliage or flower. The remainder of the container shall be planted with seasonal color to be replanted three times per year. All plant containers shall be irrigated by drip tubes on their own zone of an automatic irrigation system or on other zone dedicated to drip systems.

**Required Model for Plant Containers:**

Longshadow: Glencoe LS 9180 and LS 9085  
Natural finish



**c. Vending**

Newspaper stands shall be grouped together in a custom made black metal enclosure. Information kiosks may be designed to coordinate with the newspaper stands.



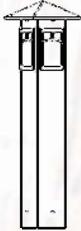
**Required Model for Vending Machines:**  
 Custom design made by Spencer Fabricate. Style to be set by first designed approved by the City.



**d. Bollards**

Bollards may be used to limit vehicular access and to light pathways.

**Required Model for Bollards:**  
 Sternberg: 720 LB-70MH-MT-VG



**e. Decorative Fountains**

Decorative fountains, if used, shall be consistent with the adopted Craftsman or Florida Vernacular styles. The size and scale of the fountain shall depend on the size of the area where it will be placed.



**f. Bike Racks**

A bike rack/water fountain combination shall be installed between the sidewalk and the on-street parking area, and shall be spaced every 1,000 feet or one per block, whichever is greater. The bike racks shall be set perpendicular to the street so that parked bicycles do not impede pedestrian circulation.

**Required Model for Bike Racks:**  
 Madrax: Classic Series  
 CLR-7-SF-PS  
 Black



**g. Drinking Water Fountains**

Drinking fountains shall be installed in combination with bike racks every 1,000 feet, or two per block, whichever results in a greater number of fountains.



**Required Model for Drinking Fountains:**  
 Most Dependable Fountains: Historical  
 Fountain  
 2002-SM  
 Powder coat black with gold bowls



**h. Benches**

Wooden benches shall be installed every 300 feet or two per block, whichever results in a greater number of benches. The benches shall be located to one side of the public sidewalk and shall be shaded.

**Required Model for Benches:**  
 Landscape Forms: Plainwell  
 72" w/ center arm  
 Natural Ipe  
 Powder coat black



**6. Lighting**

Lighting is important to ensure safety for both pedestrians and vehicles. The following standards are applicable to the entire PSJRA unless specifically noted for each sub-area.

- Exterior lighting shall be designed and installed so that it illuminates without creating glare or light spillage; safe for automobile and pedestrian traffic; and without distorting natural color.
- Neon or neon type tubing on a building may be allowed under limited circumstances provided that a lighting plan is submitted to the City for review. The neon light colors shall be consistent with, and compliment the building colors and style. Neon is not permitted on windows.
- No light fixtures shall be placed where they would conflict with the location of existing or proposed canopy trees.

**Required Model for Streetlights in Residential Areas:**

Sternberg (1)1912 G3-CAS-6-OK-30-E-OFC-175  
 PMH-MT-BK





- Electric wiring shall be provided underground.
- Lighting should conform to “dark sky” standards including downward projection, “full cut-off” fixtures that shield light from being emitted upward toward the sky or surrounding areas.

**a. Pedestrian Area Lighting**

Pedestrian scale accent lighting is required as follows:

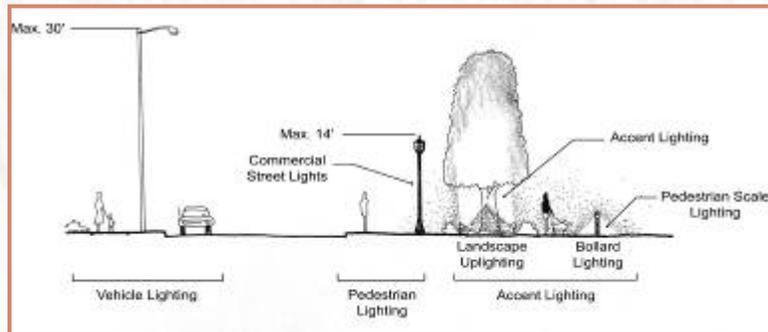
- Light fixtures may be overhead, bollards or built into walkways.
- Light poles shall not exceed fourteen (14) feet in height.
- The required minimum illumination for walkways and other pedestrian area is 0.25 footcandles.

**b. Vehicular Area Lighting**

- Light poles shall not exceed thirty (30) feet in height.
- Lighting levels shall not exceed 0.60 footcandles.

**c. Accent Lighting**

Lighting for signage, landscaping and trees, water amenities and other special features is encouraged. Concealed fixtures are preferred.



**7. Parks and Open Space**

Open spaces are recreational public spaces that may be activity based, passive or host public events and gatherings. They enhance the overall appearance and aesthetics of the community. The following regulations apply to the parks and open spaces:

- Open space requirements found in the City of Port St Joe Land Development Code shall be met.
- Small open spaces have a limited visual impact. The size of open space must be sufficient to serve the intended functional purpose. Combined required open spaces may be met several private developments together if they share parking requirements.



**Constitution Park**



- Open space will feature open green areas (vegetated).
- Parks should be adequately lit on automatic timers without projecting a glare on adjacent residential properties.
- Parks should be designed for safety and located so that they are visible from streets and surrounding homes.
- Maintenance agreement is required for all common area and open space within residential developments. Parks furniture shall be constructed with durable material and require low maintenance.
- Open space should provide shade for pedestrians via canopy trees, trellis structures or umbrellas.
- Sculptures, a decorative fountain, or public art should be provided as focal points.

## G. LANDSCAPING AND BUFFERS

Landscaping helps create a unified urban look; define outdoor spaces; buffer from sun, sound and weather; screen from view; and accentuate building elements and vistas.

Existing native trees shall be maintained to the extent possible and shall be protected during construction. When possible, service and utility easements shall be located away from mature trees (eight (8) inch DBH or larger) that can be saved.

Landscape design shall enhance architectural features, visually screen dissimilar uses and unsightly views, reduce noise impacts from major roadways, create barriers between incompatible uses, strengthen important vistas, and reinforce neighboring site design and architecture. Buffers (width and landscaping) shall not be required in front of the buildings, if they are located within fifteen (15) feet from the right-of-way.

### 1. Required Plants

Landscaping elements consist of:

- **Canopy Trees:** At the time of planting, each canopy tree shall have a minimum height of fifteen (15) feet and shall have a minimum caliper of four (4) inches measured at six (6) inches above the ground. The caliper of multi-trunk trees will be calculated using a total caliper of all trunks.
- **Understory Trees:** At the time of planting, each understory tree shall have a minimum height of six (6) feet and shall have a minimum caliper of 2.5 inches measured at six (6) inches above the ground.
- **Shrubs and Hedges:** Shrubs shall have a minimum height of twenty-four (24) inches at the time of planting and shall be thirty-six (36) inches within two years of the time of planting. When a continuous hedge is called for in this report, the hedge shall be planted at a maximum spacing of thirty-six (36) inches unless otherwise approved for individual species.

Appendix B and Appendix C contain a list of acceptable and prohibited plants.



## 2. Pruning and Maintenance

### a. Pruning

Tree pruning shall be limited to the following purposes and timeframes:

- Removal of dead, damaged, or diseased growth.
- Enhancement of a plant's natural form by selective cutting.
- Increasing the quantity or quality of foliage

### b. Maintenance

- Tree topping and “lollipopping” is prohibited on canopy trees.
- Small trees shall be pruned every three years and larger trees every six years.
- Severe pruning will result in a \$1,000.00 fine per tree and/or replacement.
- Palms shall be pruned to remove dead fronds and seed head as desired. Green fronds shall be left to maintain the tree's natural shape.
- If it is necessary to prune Crepe Myrtles, only remove branches of one (1) inch diameter or less. Larger branches only need to be removed if they are dead, damaged, or diseased.

## 3. Irrigation

All landscape and grassed areas shall be irrigated by an automatic, underground irrigation system. Heads for grass areas will be on a separate zone from landscape beds.

All plant containers shall be irrigated by drip tubes on their own zone of the irrigation system or on other zones dedicated to drip lines.

All irrigation systems will meet the state of Florida rain sensor requirements.



## 4. Walls and Fences

Fences and walls are discouraged between compatible uses. If deemed necessary, they must meet the following standards:

- Vertical elements (posts) should be incorporated into the design of the fence and shall be spaced at six (6) to twelve (12) foot intervals depending on the material and overall length of the wall or fence.
- Posts or columns may include a cap piece extending up to twelve (12) inches above the allowable fence height.
- Chain link and lattice fences are not permitted anywhere in the PSJRA.

### a. Privacy Fences and Walls

- Fences and walls shall be constructed of brick, vinyl, masonry, stone, or other decorative materials.

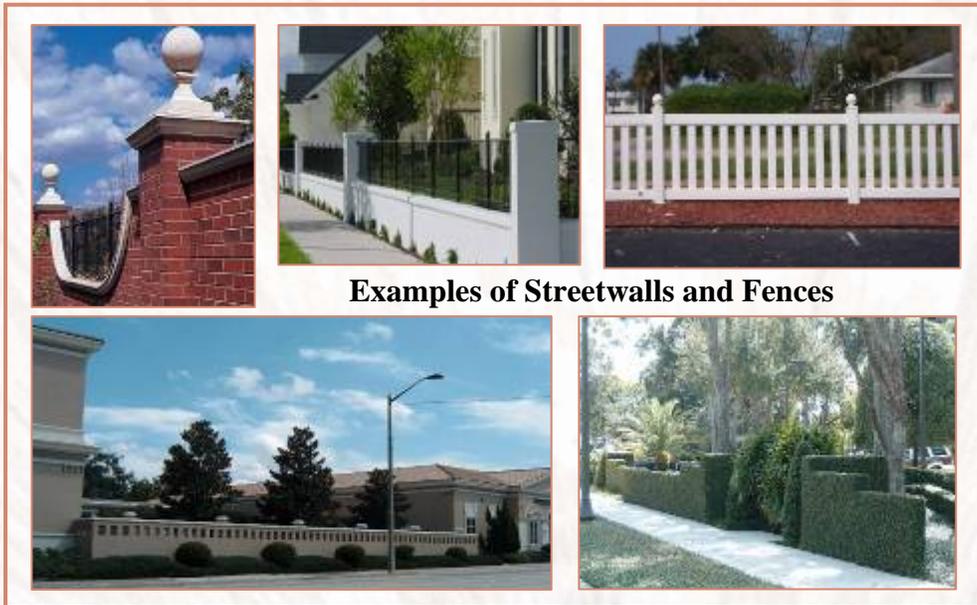
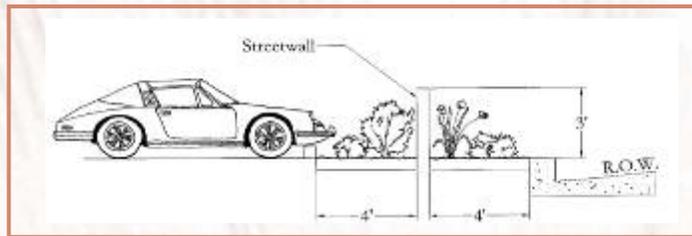


- All fences shall have their finished side facing outward.

**b. Streetwalls**

Streetwalls are required to screen off-street parking facilities (spaces or driveways) from the right-of-way.

- Streetwalls should be designed to compliment the principal building style, materials and colors.
- Streetwalls shall be a minimum of three (3) feet and a maximum of five (5) feet in height. Streetwalls greater than three (3) feet in height above grade shall be no more than fifty (50) percent solid.
- A minimum three (3) foot to five (5) foot landscaped strip shall be provided inside and outside of the streetwall. Shrubs shall be planted within the landscape area at a minimum of eighteen (18) inches in height at the time of planting, with a minimum spacing of thirty-six (36) inches on center.



**Examples of Streetwalls and Fences**

**H. STORMWATER AND UTILITIES**

**1. Stormwater**



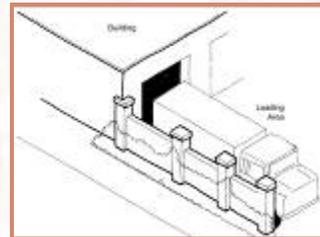
- Stormwater areas shall be designed as amenities. Features such as fountains or aerators are encouraged to accent ponds and provide adequate aeration to prevent stagnation.
- Native landscaping is required to soften the visual appearance of the pond's edges.
- The pond edges should be sloped so as to avoid the necessity of fencing.



**Example of dry retention area as amenity**

**2. Utilities/Service Areas**

- Accessory structures shall have the same architectural detail, design elements and roof design as the primary structure.
- Rooftop mechanical equipment should be integrated into the overall mass of a building by screening it behind parapets or by recessing equipment into hips, gables, parapets or similar features. Plain boxes as the only screening mechanism are not acceptable.
- Equipments installed at ground level shall be screened by low walls or landscaping.
- Areas for outdoor storage, trash collection, and loading shall be incorporated into the primary building design. The materials used shall be of comparable quality and appearance as those of the primary building.
- Loading areas or docks, outdoor storage, waste disposal, mechanical equipment, satellite dishes, truck parking, and other service support equipment shall be located behind the building line and shall be fully screened from the view of adjacent properties both at ground and roof top levels.
- Where storage/utilities/loading facilities are located adjacent to residential lots, they shall include a visual and acoustic buffer.
- When the loading and/or refuse collection area is adjacent to a residential district, deliveries and collections shall not occur between 10 p.m. and 7 a.m.
- Signage shall be installed prohibiting vehicular idling in areas adjacent to residential lots.
- Shopping cart storage shall be located inside the building or shall be screened by a four (4) foot wall consistent with the building architecture and materials or a solid landscaping hedge.
- Large fenced areas such as garden centers and home improvement supply yards, adjacent to or behind the building line, that are visible from the public right-of-way shall be screened with vinyl-coated chain link or other decorative fencing material. The area shall be landscaped and designed to be compatible with the design of the building façade.





- Utilities for all new developments and redevelopment shall be located underground.

**I. SIGNS AND MURALS**

**1. Signs**

The following standards shall apply to all businesses within the PSJRA. If any of the provisions noted in this chapter conflict with the current sign code, the most restrictive shall apply.

**a. Freestanding Signs**

- Monument signs should only be allowed if the building is setback more than fifteen (15) feet from the front property line, regardless of the site width.
- Signs should identify an individual business or the complex. No multi-tenant signs shall be allowed.
- The design of the monument sign shall be architecturally compatible with the style, composition, materials, colors and level of detail of the principal structure.

**b. Attached Signs**

- Signs shall not cover architectural detailing, windows or building ornamentation
- Pedestrian signs placed under a canopy or arcade perpendicular to the street shall be exempt from signage requirements as long as they do not exceed eight (8) square feet in area.
- Where possible, signs located on buildings within the same block shall be placed at the same height in order to create a unified sign band.
- Wall signs shall not exceed seventy-five (75) percent of the width of the wall where they are placed, and shall not exceed 10% of the square footage of the façade where the sign is placed.
- The copy area of canopy or marquee signs should not exceed seventy-five (75) percent of the canopy or awning face vertically or horizontally.
- Awning signs shall not be placed on the curved or diagonal portion of the awning. The width of the sign shall not exceed seventy-five (75) percent of the width of the awning.
- Marquee signs shall be erected only within the face of the marquee and shall not extend outside the edge of marquee.

**2. Murals**

One (1) mural shall be allowed per establishment subject to the following standards:

- The mural shall be placed only on walls with no doors or windows or architectural details/ornamentation. They require approval from the City to ensure the artwork does not





constitute a sign otherwise permitted or prohibited and that the artwork complements the design of the building in color, shape and location.

- The mural area shall not exceed the square footage of the façade.
- No mural shall be placed closer than two hundred (200) feet to any single-family residential development.



## V. PSJRA DOWNTOWN STREETS AND MLK BOULEVARD

The PSJ downtown area (Reid Avenue, Williams Avenue, 2nd Street, 3rd Street, 4th Street) and properties fronting on Martin Luther King Boulevard present a “main street” development pattern. Some of the characteristic features of main street-type development include smaller city blocks, buildings close to the street, pedestrian oriented environments, rear alleys, on-street parking (currently only on Reid Avenue), and a mixture of land uses. To maintain and enhance the existing pedestrian oriented character of the PSJRA downtown standards for streetscape furnishing, right-of-way development, and building design were developed.

Properties within the downtown area or fronting on Martin Luther King Boulevard are required to adopt the following standards in addition to the general guidelines noted in the previous section.

### A. EXISTING CONDITIONS

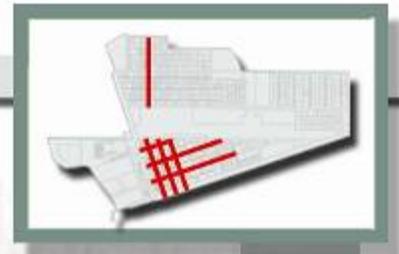
#### 1. PSJRA Downtown

The PSJRA downtown is bound by 1st Street on the north, US 98 on the west, and 5th Street on the south. Reid Avenue, Williams Avenue, Long Avenue and Woodward Avenue are the main north-south corridors; and 2nd Street, 3rd Street and 4th Street are the main east-west corridors in the downtown. The buildings located in the downtown area are one to two stories high with a typical setback of zero feet. Florida Vernacular appears to be a preferred style of existing development.

The entire downtown area has been designated as commercial in the City’s Future Land Use Map (Comprehensive Plan). The existing land use is predominantly commercial with some public uses along Reid Avenue, Williams Avenue and Long Avenue, and predominantly residential along Woodward Avenue. There are a lot of vacant parcels located in the downtown area that provide ample opportunity for future development. The downtown has been proposed as a Mixed Use district in the PSJ Redevelopment Agency Conceptual Master Plan.

The existing roads in the downtown area are pedestrian oriented with maximum speed of 25 miles per hour. The downtown area has a system of twelve foot (12) wide alleyways running behind all the main streets providing service access to the buildings. All the main streets in the downtown area have different right-of-way widths: Reid Avenue has a right-of-way width of fifty-three (53) feet and Williams Avenue has a right-of-way of forty (40) feet.

There are no existing sidewalks or streetscape improvements in the downtown area, except along Reid Avenue. Reid Avenue has nine (9) foot wide concrete sidewalks running along both sides of the Avenue, crosswalks at every intersection, light poles with banners, trash cans, vending machines, and benches. Reid Avenue also has on-street angled parking on both sides of the street. Landscape islands separate the parking spaces from the sidewalks and are planted with palm trees and a mixture of shrubs and grasses.



There are no parking garages in the downtown area. The Dr. Joe Hendrix Memorial surface parking lot, which was constructed at Williams Avenue and 14th Street as a part of the continuing improvements in the PSJRA, provides 35 parking spaces including two (2) handicap accessible spaces.



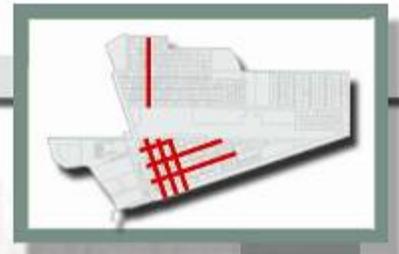
## 2. Martin Luther King Boulevard (MLK)

Martin Luther King Boulevard runs north south in the north side of the PSJRA. The width of paved surface area on MLK Boulevard is approximately forty-one (41) feet, and the actual pavement width is thirty-one (31) feet. The existing land uses along MLK between Avenue A and Avenue D are commercial and public, and between Avenue D and Avenue G the land use is residential. MLK Boulevard has been proposed as a Mixed Use corridor in the PSJRA's conceptual Master Plan.



Buildings along MLK Boulevard are one (1) to two (2) stories high located right up to the five (5) foot concrete sidewalk. A twelve (12) foot wide alleyway runs perpendicular to the MLK Boulevard serving as a rear entrance to the buildings on Avenue A to Avenue G. There are no designated parking spaces along the street, and there are no curb cuts, benches, trash receptacles, or landscaping. Cars currently park anywhere within the MLK right-of-way, frequently blocking the sidewalk areas.

There are some existing streetlights and utility poles with exposed wiring on both sides of MLK Boulevard.



Florida Vernacular seems to be the preferred style of existing development.

**B. DESIGN STANDARDS**

In addition to the General Design standards listed in Section IV, all parcels within the PSJRA downtown area and properties fronting on Martin Luther King (MLK) Boulevard must adhere to the following requirements.

**1. Building Height**

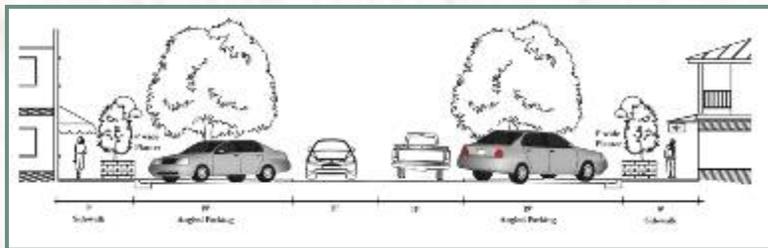
New development and redevelopment should be designed to be respectful to the existing vertical height of adjacent buildings, and should maintain the pedestrian scale. The building height within this area shall be a maximum thirty-five (35) feet.

**2. Design of Public Realm**

**a. Right-of-Way Design and Parking**

All streets in the downtown PSJRA shall meet the general standards listed in **Section IV.F.1**, except for the following:

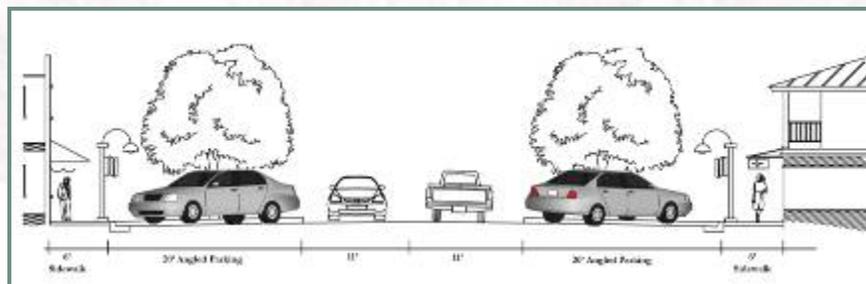
**(1) Reid Avenue**



Based on a current right-of-way width of approximately seventy-eight (78) feet, Reid Avenue will be redesigned to match the two-lane street with angled parking cross-section noted in **Section IV.F.1.b**, except for the following variations:

- **Curb and Gutter:** The curb and gutter shall be included within the parking space width.
- **Planters:** Four (4) feet wide planter placed adjacent to the curb.
- **Sidewalk:** Five (5) feet wide (measured from the planter).

**(2) Williams Avenue**

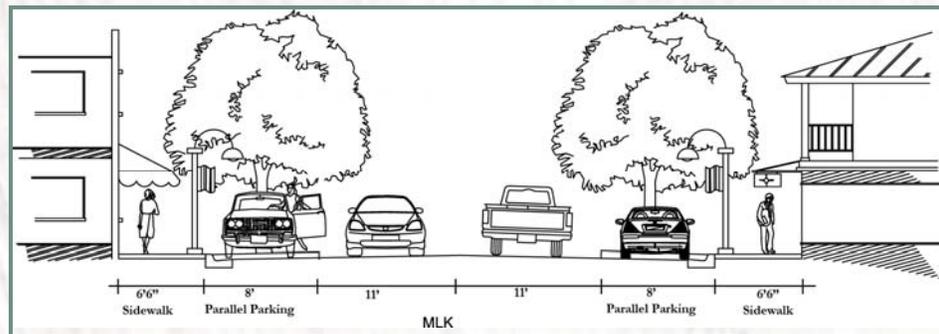




The cross-section for Williams Avenue, which currently measures approximately seventy-four (74) feet in width, shall match the two-lane with angled parking design except for the following features and dimensions:

- **Curb and Gutter:** The curb and gutter shall be included within the parking space dimension.
- **Sidewalk:** Six (6) foot wide sidewalks adjacent to the curb.

**(3) MLK Boulevard**



MLK Boulevard, with a current right-of-way width of fifty-one (51) feet, shall be designed as a two-lane roadway with parallel parking as noted in **Section IV.F.1.a**, except for the following features and dimensions:

- **Curb and Gutter:** The curb and gutter shall be included within the parking space dimension.
- **Sidewalk:** Six and a half (6.5) feet wide adjacent to the curb.

**3. Site Furnishings**

In addition to the site furnishings required in Sections IV.F, development along the downtown streets must meet the following standards:

**a. Lighting**

- Street light with decorative fluted poles and scrolled candy cane arms (to match the existing street lights along Reid Street) shall be installed on all the streets.
- Decorative street lamps are to line both side of the street at a spacing to achieve 0.6 average foot-candles of illuminations.
- The height of the light pole shall be maximum fifteen (15) feet.
- Every other light will feature a hanging basket planted with seasonal color and vines alternating with banners.
- Drip tube for irrigating the baskets will be routed internally through the pole and planter arms.



- Since the irrigation is plumbed internally, hanging baskets are not recommended for existing streetlights.

**Required Model for Streetlights:**

Sternberg: (1) 1910/5 RLM 18/508 6214  
RPF/CC3639/175 MH-BK

Alternate DBA- xx/ (1) PA478 and PM-MOD-DI.



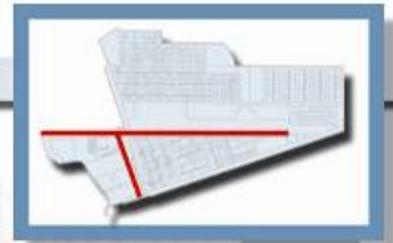
**b. Landscaping**

- **Brick Planters:** Two understory trees (four-inch minimum caliper) shall be planted within each brick planter, alternating the use of two Little Gem Magnolias with two Crepe Myrtles. The planters shall be completely landscaped with shrubs, groundcovers and/or seasonal color (annuals and perennials). See approved plant list (**Appendix B**).
- **Landscape Islands:** A 'Highrise' Live Oak tree (4" minimum caliper) shall be planted in each landscape island ("bump out").



**c. Banners**

Banners shall be installed on street light pole alternating with hanging baskets. The banners shall match the ones on Reid Street.



**VI. MARINA DRIVE/EAST 1ST STREET AND BALTZELL AVENUE**

**A. EXISTING CONDITIONS**

**1. Marina Drive/East 1<sup>st</sup> Street**

Marina Drive/East 1st Street is the main east-west corridor through the redevelopment area. The portion between the Gulf of Mexico and Clifford Simmons Parkway is known as Marina Drive. The segment east of Clifford Simmons is known as East 1<sup>st</sup> Street. The two-lane roadway is mostly paved, except for its easternmost segment, which is still unpaved. Marina Drive has sidewalks along the south side, but none on the north side. East 1st Street has virtually no sidewalks on either side.

The roadway width appears to be approximately twenty-five (25) feet. The East 1st Street section has the potential to be widened. However, Marina Drive is constricted because of several buildings located within a few feet of the street.

The south side of the street is mostly developed. However, the opposite is true for the northern side, which contains only a few buildings. Most of the newly constructed buildings along this corridor, such as the Bayside Bank and some of the new residences, have been designed with a Florida Vernacular architectural style. The building setbacks vary from building to building. The average building setback appears to be between fifteen (15) to twenty (20) feet. However, some of the buildings appear to fall outside of this range.



*Bayside Bank*

One of the main buildings along the East 1st Street/Marina Drive corridor is the old railroad office building. This historic structure features a red brick façade with a flat roof. This building has the potential to be revitalized and can be converted into a residential or mixed-use.

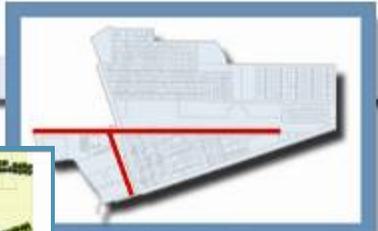


*Old Railroad Office Building*

Marina Drive (East 1st Street) has three distinct areas based on the future land use assigned to each segment:

- The eastern segment, categorized as predominantly industrial and heavy commercial areas in the Redevelopment Plan Land Use Map.
- The western portion comprised of the Port St. Joe Marina and commercial/office uses.
- The middle segment designated for mixed-use.

Because of the unique nature of these segments, the design standards have been drafted according to location and future land use.



## 2. Baltzell Avenue

Baltzell Avenue runs north-south in the west side of the PSJRA, parallel to US 98. The existing land uses along Baltzell Avenue are commercial and public. A mixture of uses has been proposed along Baltzell Avenue in the PSJ Redevelopment Agency Conceptual Master Plan; commercial and office on the east side; and park, single and multi family, and commercial and office on the west side.

There are buildings on the east side of Baltzell Avenue, while the west side is undeveloped. Buildings along Baltzell Avenue are one (1) to two (2) stories high and the setbacks vary from eight (8) feet to twenty-six (26) feet. A twelve (12) foot wide alleyway runs perpendicular to Avenue serving as a rear entrance to the sites fronting Baltzell. There are no designated parking spaces along the street, curb cuts, benches, trash cans, or landscaping.

There are some existing streetlights and utility poles with exposed wiring on both side of Baltzell Avenue. Florida Vernacular seems to be the preferred style of existing development.

### B. DESIGN STANDARDS

In addition to the non-residential general design standards, any parcel within the PSJRA that has frontage on Marina Drive/East 1st Street or Baltzell Avenue must adhere to the following requirements.

#### 1. Building Placement

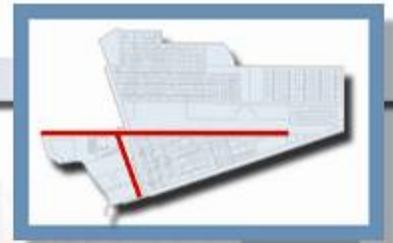
Building placement along this corridor is a crucial component to the impact of industrial and heavy commercial uses on less intensive uses in the area.

##### a. Front Yard Setback:

Front yard setbacks shall be as follow:

##### (1) Marina Drive/East 1st Street:

- Industrial and heavy commercial uses shall have maximum front yard setback of twenty (20) feet
- Other uses such as mixed-use, commercial and marina areas shall be built up to the property line.



- (2) **Baltzell Avenue:** New development shall be built up to the property line.

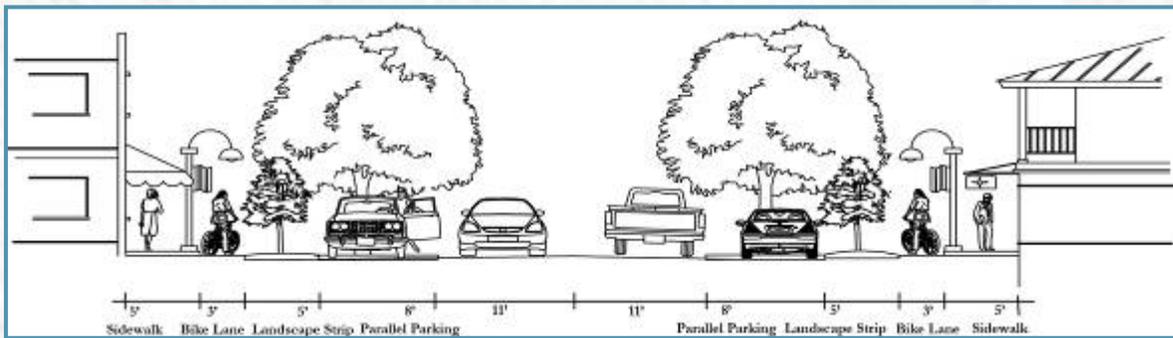
**b. Side Yard Setbacks:**

Side yard setbacks contribute to the block continuity and provide additional buffering from incompatible uses. The side yard setbacks shall be applied based on the type of use and shall be as follows:

- (1) **Industrial or heavy commercial use:** The side yard setbacks shall be a minimum of fifteen (15) feet from the property line. However, side yard setbacks shall not apply to campus industrial/heavy commercial sites, which require assemblage of parcels.
- (2) **Other Uses (mixed-use, commercial and marina uses):** Side yard setback for the development along the main roads should be determined based on the existing development on the adjacent lots as noted in the general standards section:

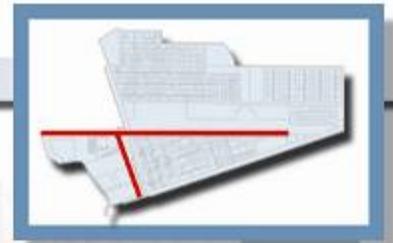
**2. Right-of-Way Design**

Based on a current right-of-way width of approximately sixty-four (64) feet, Marina Drive/1<sup>st</sup> Street and Baltzell Avenue will be redesigned to match the two-lane street with parallel parking cross-section noted in **Section IV.F.1.a**, except for the following variations:



- **Curb and Gutter:** The curb and gutter shall be included within the parking space dimension.
- **Sidewalk:** Five (5) foot wide sidewalk on both sides of the street. Sidewalks shall be constructed of broom finished concrete (see general standards section for specifications), except along **Marina Drive West of US 98 and Baltzell Avenue**, which shall be **Tabby Concrete** (see picture), which is concrete with a top dressing of crushed oyster or scallops shells seeded in. It provides added texture, provides historical context and ties in the waterfront theme.





- **Landscaping Strip:** A five (5) foot wide landscape strip, meeting the standards noted in the general section (**Section IV.F.1.a**), shall be installed separating the parking bay and the bike lane.
- **Bike Lanes:** Required on both sides of the street between the landscape strip and the sidewalk, with a minimum width of three (3) feet.

**3. Site Furnishings**

In addition to the site furnishings required in Sections IV.F, development along Marina Drive/1<sup>st</sup> Street and Baltzell Avenue must meet the following standards:

**a. Lighting and Banners**

- Streetlights shall be installed along Marina/1<sup>st</sup> Street and Baltzell on both sides of the street to achieve 0.6 average footcandles of illumination.
- The height of the light pole shall be maximum fifteen (15) feet.
- Every other light will feature twin-hanging basket planted with seasonal color and vines alternating with banners. Drip tube for irrigating the baskets will be routed through the pole and planter arms.
- The streetlight specifications for 1<sup>st</sup> Street east of US 98 shall be different than the area west of US 98 and Baltzell Avenue to reflect the different characters of these two areas.

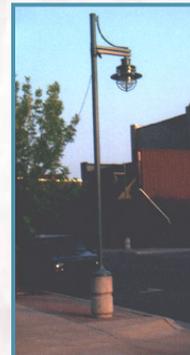
**Required Model for Streetlights along 1<sup>st</sup> Street east of US 98:**

Sternberg: (1) 1910/5 RLM 18/508 / 6214RPFP/CC3639 scrolls/175 MH-BK, alternate DBA-xx / (1) PA 478 and PM-MOD-DI.

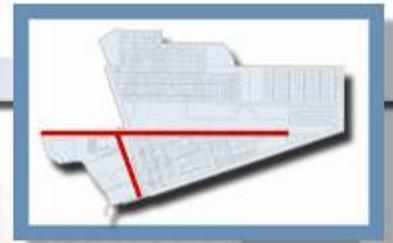


**Required Model for Streetlights Along 1<sup>st</sup> Street west of US 98 and Baltzell Avenue:**

Sternberg (1) 1480 CA-L03G-OG-175MH-550-P-15'-BK



Marina Dr. / 1st St. / Baltzell Ave.

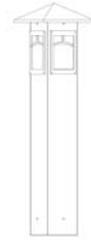


**b. Bollards**

If bollards are desired in addition to street lamps to light up pedestrian areas and to add to the aesthetic value of the landscaping, the following styles may be used.

**Required Model for Bollards along 1<sup>st</sup> Street east of US 98:**

STERNBERG 720LB- 70MH-MT-VG



**Required Model for Bollards along 1<sup>st</sup> Street west of US 98 and Baltzell Avenue:**

Bega 9147 MH (70 MN)-BLK



**c. Benches**

Benches shall be located to one side of the public sidewalk and shall be shaded. The style shall match the specifications noted in the general standards section (Section IV.F.5 , except for the area along 1<sup>st</sup> Street west of US 98 and Baltzell Avenue, which shall used the following model:

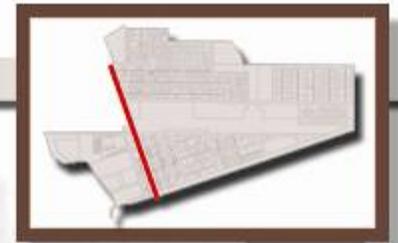
**Required Model for Benches Along 1<sup>st</sup> Street west of US 98 and Baltzell Avenue:**

DuMor 131-60 PL- Grey recycled plastic slats, Stainless Steel w/ matte finish supports.



**4. Landscaping**

A ‘Highrise’ Live Oak tree (4” minimum caliper) shall be planted in each landscape island ("bump out"). Properties fronting Marina Drive, west of US 98 to the Marina shall be planted with Sabal Palms in the landscape strips. Trees shall be spaced every twenty five (25) feet on center along both sides of the street.



**VII. US 98**

US 98 is the major north south corridor connecting the City of Port St Joe with the surrounding area. This Federal Highway primarily runs along the Gulf of Mexico and serves as a primary hurricane evacuation route. Since this highway carries high volumes of traffic, the commercial uses along the highway have a primary focus on marketing to vehicular traffic, rather than pedestrians or bicyclists. In the CRA, US 98 extends from Avenue “D” to 5th Street.



**A. EXISTING CONDITIONS**

The existing right-of-way of approximate seventy (70) feet accommodates on street parallel parking in some areas. Five (5) foot concrete sidewalks are provided on both sides of the street. Crosswalks are either unmarked or stripped with white paint. There are no existing streetscape furnishings or canopy trees within the ROW. The landscape strip, adjacent to the travel lanes, is inconsistent in width and composition. Some areas are paved, while others are grassed. Building heights vary from one to three stories and setbacks vary from ten (10) to more than fifty (50) feet.

Florida Vernacular seems to be the preferred style of new development. The Port Side Inn, a historic building in the Florida Vernacular style, fronts on US 98.

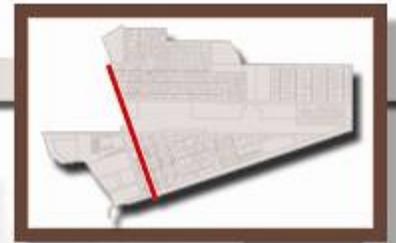
**B. DESIGN STANDARDS**

In addition to the general design standards noted in **Section IV**, any parcel fronting on US 98 must adhere to the following requirements.

**1. Building Setbacks**

Allowing buildings along US 98 to be placed too far from the front property line creates a large gap between the pedestrian and the business. This gap is typically where the parking has been located in current development. This building/parking placement creates an uncomfortable walking environment for a pedestrian and encourages shoppers to use their vehicles to go from one merchant to the next. Similarly, excessive front yard setbacks often contribute to sign proliferation and visual blight because a building that is set back a large distance often reacts by “shouting”, with signs, at passing motorists and transit users, bicyclists, and pedestrians in order to be noticed.

Where the parking is located in front of the building, the parking area should be designed to avoid the appearance of large masses of pavement, and shall be conducive to pedestrian access and circulation. The maximum front yard setback along US 98 shall be eight (80) feet, measured from the public sidewalk. The 80-foot setback may include one bay of parking, building foundation, landscaping, street wall, and internal sidewalks.



**2. Right-Of-Way Design**

- US 98 is a State road and as such must meet FDOT design standards. If possible, the on-street parallel parking spaces between Avenue D and Eighth Street shall be maintained.
- The landscape strip shall contain the following landscaping materials:
  - A ‘Highrise’ Live Oak trees (4” minimum caliper) planted every fifty (50) feet on center.
  - Twenty one (21) shrubs (1 gallon) planted in a mass at the base of every other tree.
  - The shrub masses shall alternate between Muhly Grass (Muhlenbergia capillaries) and Blue Flax Lily (Dianella species ‘Silver Streak’).
  - Sod is to span the distance between shrub masses.

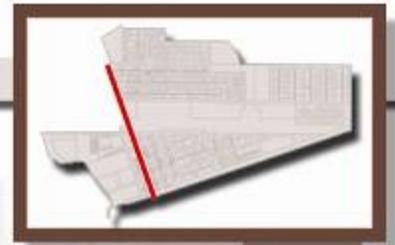
**3. Street Walls**

In addition to the requirements of the street wall listed in **Section IV.G.4**, the following standards apply to street walls along US 98:

- Street walls shall be located in the landscaping buffer provided between the public sidewalk and on-site parking.
- A minimum of five (5) foot landscaped area shall be provided outside of the street wall.
- A minimum of three (3) feet of landscaped area shall be provided inside of the street wall.

**4. Lighting**

Decorative street lamps are to line both sides of the street at a spacing to achieve an average illumination of 1.5 footcandles. Depending on the total right-of-way width, light poles of thirty (30) feet could be spaced one hundred (100) feet apart. The fluted aluminum poles, while taller than those proposed for local streets, will match in color and style. All lamps shall have Night Sky optics and cut-off luminaries to reduce light pollution.

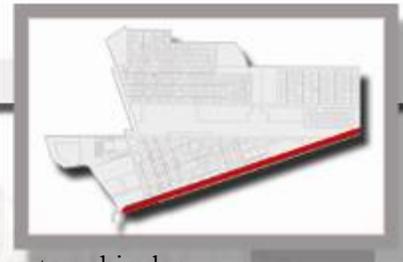


**Required Model for Streetlights:**

Sternberg: 1913G3/FFA6-HS/  
9230TFP8/ 175 MH/ROH 3/ RDBP/  
BK (black)



US 98



**VIII. SR 71/5th STREET**

State Road 71 is a major east/west corridor in the PSJRA. Since this highway carries higher volumes of traffic it has a primary focus on marketing to vehicular traffic, rather than pedestrians or bicyclists.

**A. EXISTING CONDITIONS**

The approximate one hundred and fourteen (114) foot ROW accommodates on street parallel parking on the west end of the street near downtown. A seven (7) foot concrete sidewalk currently extends along both sides of the road near downtown. The sidewalk ends on the north side near Park Avenue. CR 71 is the only existing road in Port St. Joe with a landscaped median. The center median is thirty (30) feet wide. With this median and seven (7) foot wide landscape strips alongside this road, SR 71 possesses some of the widest green space, albeit lightly landscaped, in town. Both of these factors would make SR 71 a prime candidate for a landscape renovation.



Existing street trees along SR 71 include Bradford Pears and Sabal Palms. The Bradford Pears, known to be a tree with weak branching, do not have dense, consistent canopies. The Sabal Palms are being improperly pruned as shown in this photo. See Maintenance/Pruning in the General Section of this report.

Building heights vary from one (1) to three (3) stories, and setbacks vary from about eight (8) feet on the west end of town to more than two-hundred (200) feet on the east end of town. Florida Vernacular seems to be the preferred style of architecture.

**B. DESIGN STANDARDS**

In addition to the non-residential general design standards, any parcel in the PSJRA fronting SR 71 must adhere to the following requirements.

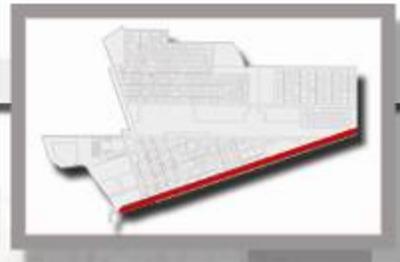
**1. Building Setbacks**

Allowing buildings along SR 71 to be placed too far from the front property line creates a large gap between the pedestrian and the business. This gap is typically where the parking has been located in current development. This building/parking placement creates an uncomfortable walking environment for a pedestrian and encourages shoppers to use their vehicles to go from one merchant to the next. Similarly, excessive front yard setbacks often contribute to sign proliferation and visual blight because a building that is set back a large distance often reacts by “shouting”, with signs, at passing motorists and transit users, bicyclists, and pedestrians in order to be noticed.

Where the parking is located in front of the building, the parking area should be designed to avoid the appearance of large masses of pavement, and shall be conducive to pedestrian access and circulation.

The maximum front yard setback along SR 71 shall be eight (80) feet, measured from the public sidewalk. The 80-foot setback may include one bay of parking, building foundation, landscaping, street wall, and internal sidewalks.

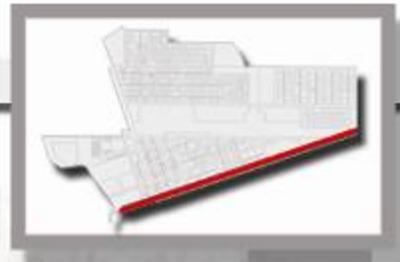
State Road 71



## 2. Right-of-way Design

- As in the case of US 98, SR 71 is a State roadway and shall meet the design standards of FDOT. If feasible, on-street parking should be accommodated within the existing right-of-way from US 98 to Woodward Avenue.
- The landscaping strip adjacent to the parallel parking along right-of-way shall conform to the following standards:
  - All the existing Bradford Pears should be removed. The trees will only continue to decline due to their weak branching.
  - ‘Highrise’ Live Oak trees, four (4) inch minimum caliper, shall be planted every fifty (50) feet on center in the landscape strips.
  - After approximately twenty (20) Live Oaks (depending on block length) tree planting shall be changed to Allee Elms. Allee Elms shall also be a four (4) inch minimum caliper size and shall be planted every fifty (50) foot on center in the landscape strips.
  - Twenty one (21) shrubs (1 gallon) shall be planted in a mass at the base of every other tree. The shrub masses will alternate between Muhly Grass (*Muhlenbergia capillaries*) and Blue Flax Lily (*Dianella species ‘Silver Streak’*).
  - Sod is to span the distance between shrub masses.
- The landscaping median in the center of right-of-way shall conform to the following landscaping standards:
  - Two Crepe Myrtle trees shall be planted side by side between each existing Sabal Palm.
  - After approximately twenty (20) Crepe Myrtles (depending on block length) Nellie Stevens Hollies shall be planted at the same spacing. See Maintenance/Pruning in the **Section IV.G.2** of this report for the proper pruning of Crepe Myrtles.
  - The understory trees and Sabal Palms shall be grouped together in undulating landscape beds.
  - A combination of Agapanthus, Knock Out Roses, Bulbine, Dwarf Yaupon Holly, Cardboard Palm, and Lantana shrubs shall fill the landscape beds.

State Road 71



- All shrubs shall be a minimum of three (3) gallon size except for one (1) gallon Agapanthus, Bulbine and Lantana.
- Shrub beds shall be alternate with groundcover beds consisting of ‘Emerald Goddess’ Liriope.
- Southern Magnolias shall also be planted in the center median where there are no existing Sabal Palms. They shall be a four (4) inch minimum caliper size and planted every sixty (60) feet on center.

**3. Street Walls**

In addition to the requirements of the street wall listed in **Section IV.G.4**, the following standards apply to street walls along SR 71.

- Street wall shall be located in the landscaping buffer provided between the public sidewalk and parking.
- A minimum of five (5) foot landscaped area shall be provided outside of the street wall.
- A minimum of three (3) feet of landscaped area shall be provided inside of the street wall.

**4. Lighting**

Decorative street lamps are to line both sides of the street at a spacing to achieve an average illumination of 1.5 footcandles. Depending on the total right-of-way widths, light poles of thirty (30) feet could be spaced one hundred (100) feet apart. The fluted aluminum poles, while taller than those proposed for local streets, will match in color and style. All lamps shall have Night Sky optics and cut-off luminaries to reduce light pollution.

**Required Model for Streetlights:**

Sternberg: 1913G3/FFA6-HS/  
9230TFP8/ 175 MH/ROH 3/ RDBP/  
BK (black)



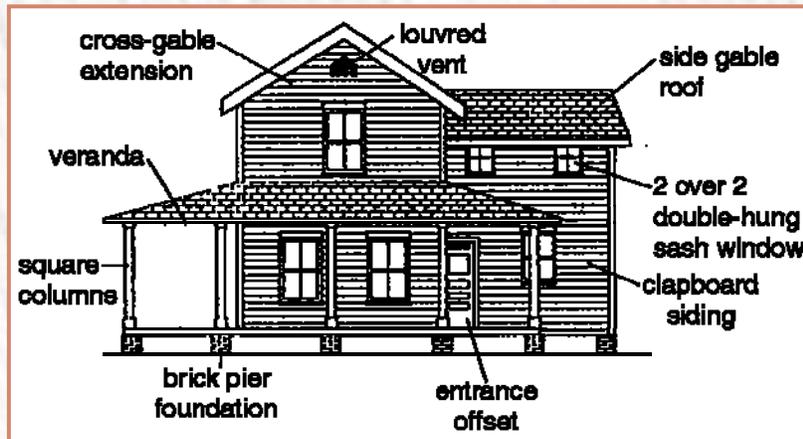
State Road 71

**IX. APPENDIX A**

**ARCHITECTURAL STYLES**

Florida Vernacular and Craftsman styles are the two preferred styles for the PSJRA. The following section lists the characteristic features of each style.

**A. FLORIDA VERNACULAR**

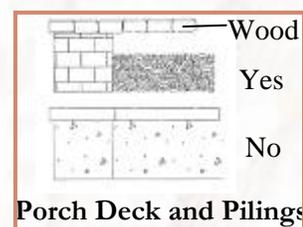


**1. Plan**

- Simple building forms.
- Rectangular and L-shaped building plans, although some buildings have irregular plans.
- Buildings range from one to two-and-one-half stories.

**2. Foundation**

- Brick or concrete block pier foundation.
- Spaces between piers left open to allow for ventilation and for protection from high water.
- Lattice infill between piers.



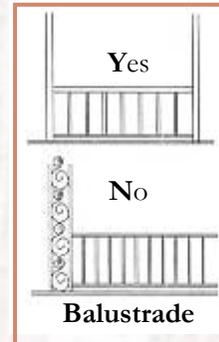
**3. Façades**

- Façades are vertically proportioned following a three bay or 5-bay pattern on the front elevation.
- Most commonly simple entrance or end porches.

**4. Porches**

- Wide front porches.
- Porches can be full width, wrap around, front facing gable, or fill in between the “L” formed by the main body and the front gable.
- Porches normally have a minimum depth of six (6) feet.

- Porches are commonly elevated 2'-6" to 3'-6" above grade.
- Columns typically narrow and made of wood; usually spaced evenly across the façade, with few details.
- In most cases, porches were built without railings. If railings were used, they were wooden with 1 ¼ inch square balustrade.



**5. Roof**

- Front gable; side-gable; and cross gable.
- Shed roof over porch.
- Earlier period homes have steep pitches, to accommodate attic space.
- Later period homes have a lowered roof pitch.
- Main roofs are steeply pitched (8:12 to 12:12). Porch roofs should have a low pitch (2:12 to 4:12).
- Rafter ends are unadorned, exposed, and extend beyond the face of the wall.
- Wood shingles were often used to cover the roofs in early homes.
- Asbestos shingles, composition shingles, V-crimp metal or metal shingles were used on later period structures, or as a replacement roof material.
- Brick or stucco chimneys, normally on the outside of the gable end walls.
- Dormers sometimes used on 1 ½ story homes.

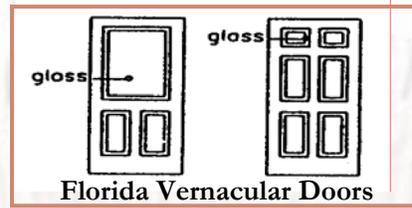
**6. Exterior**

- Primary exterior material is horizontal wood siding; less common are wood shingles and board and batten.

**7. Windows & Doors**

- Windows are single, tall and narrow.
- Multi-pane, double-hung sash windows (6/6, 3/1, 2/2 or 1/1).
- Transoms, fanlights and attic louvers are common.
- Second floor windows align with first floor windows.
- Jalousie windows, French doors and simple balconies are used occasionally.
- Windows made of wood.
- Windows are spaced evenly along all façades.

- Window and door trim projects out from wall cladding, approximately 3/4". Jamb trim is at least 4" wide, and headers (lintel) are normally 6" wide.



- Doors contain recessed wood panels.
- Shutters are not typically used. When used, they should be paneled and should be either operable or proportioned to look operable.

**8. Color**

- Colors range in the pastel family from light whites, yellows and grays to light pastel colors.

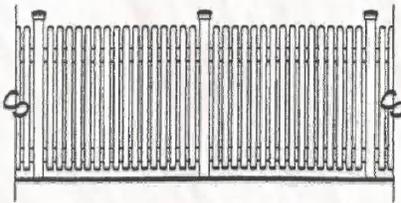
**9. Exterior Decoration**

- Sparse, limited to ornamental woodwork.

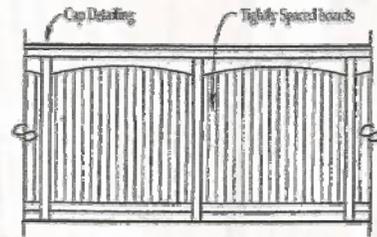
**10. Fences**

- Wooden fences are most common.
- Picket fences with various level of ornamentation, consistent with the main building.

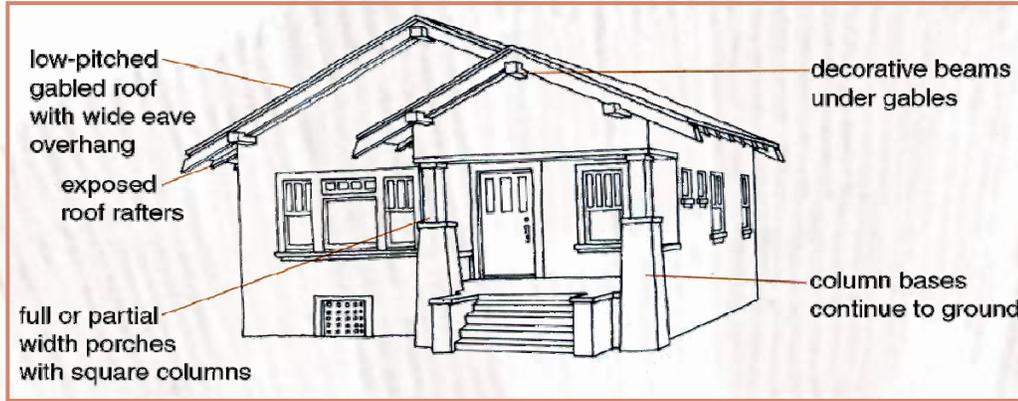
**Preferred style for Front Yard**



**Preferred style for Rear and Side Yards**



**B. CRAFTSMAN**



**1. Plan**

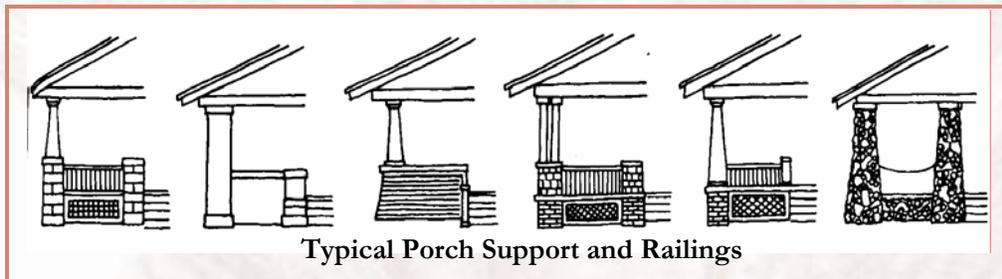
- Usually rectangular, with the narrow side oriented toward the street.
- One or one-and-a-half story. In some, cases two stories.

**2. Foundation**

- Masonry (often brick) piers or continuous brick or concrete block. Piers occasionally flared at the bottom.
- Spaces between piers left open to allow for ventilation and for protection from high water.
- Lattice or louvered panels cover the porch crawlspace.

**3. Porches & Façades**

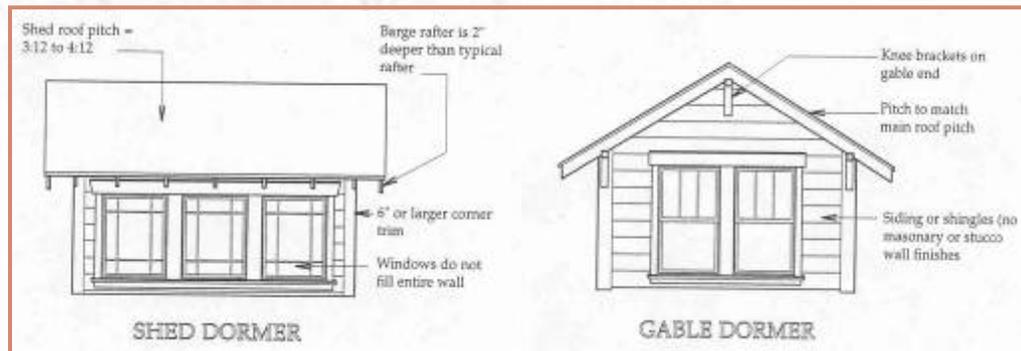
- The front porch is an essential element of all Craftsman Style houses. They are often the most prominent architectural feature of the house.
- Either full or partial width (usually  $\frac{3}{4}$  of the body width min.), with roof supported by tapered square columns.
- Partial porches are placed at the center of the main body or fully to one side.
- Elevated 2'-6" to 3'-4" above grade.



- Porches are attached to the main façade of the house, sometimes wrapped around the side.
- Porches are wide and deep enough to feel like an outside room.
- Porch wide beams help define the horizontal proportions of the style.
- Columns or pedestals frequently extend to ground level (without a break at level of porch floor).
- Columns are made of wood, concrete, or masonry. Moldings are normally found at top and bottom of column.
- Solid knee walls, matching column bases, are used between the column bases.
- Railings and balusters are occasionally used. Open, heavy wood appearing with regular or irregular pattern.

**4. Roof**

- There are four types of roofs associated with bungalow.
- Hip roof over one-and-a-half story, with a shed dormer on the main façade.



- One or more gables perpendicular to the street, with one being the most dominant, usually above the porch.
- Gable parallel to the street with cross gable intersecting; cross gable typically covers the front porch and entrance to the building.
- Large one-and-a half story home with a gable parallel to the street and dormer.
- Low- to moderately-pitched roofs, emphasizing the horizontal massing of the style (4:12 to 6:12 pitch in one to two story gabled; 7:12 to 9:12 pitch for one and 1 ½ story). Integral porch roof may match main pitch, or break at front wall to a 3:12 or 4:12 pitch.
- Exposed rafters. Rafter ends extend beyond the face of the wall, often decoratively cut.

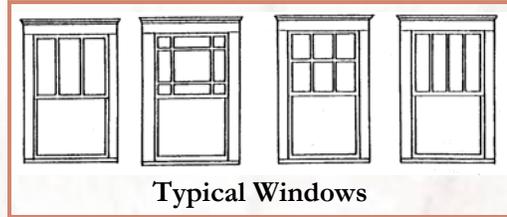
- Decorative (false) beams or braces commonly added under gables.
- Roof coverings may be wood, composition, or metal shingles, or crimped metal panels. Shingles are often patterned.
- Shed or gable dormers on the front side of the roof on the 1 ½ and 2 ½ story homes.
- Chimneys are typically brick, to match the column base, with simple decorative caps.
- Tapered chimneys on the outside of gable wall.

**5. Exterior**

- Wood siding, shingle and clapboard are the most common exterior wall surface materials. Corner trim used with siding.
- Stucco and brick less frequently used.
- Different siding on the first and second floors – wooden clapboards on the first floor and wooden shingles on the second – although only one material is also common.

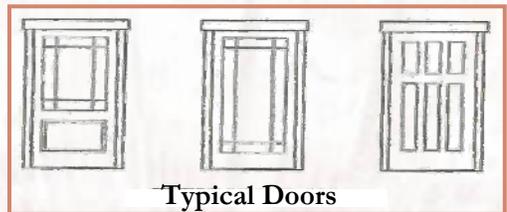
**6. Windows & Doors**

- Windows often grouped in clusters of two or three windows.
- Windows can be single-pane, 2- or 4-pane; the upper sashes may be multiple-pane (vertical) with the lower sashes single pane.



**Typical Windows**

- Dormer windows do not cover the full width of the dormer.
- Shutters not used.
- Window trim is thick to project out from the wall.



**Typical Doors**

- Windows are large and wide proportioned.

- Wood windows, either single or double-hung, or casement sash.
- Simple doors, often with panes of glass. No transom or sidelights used.

**7. Color**

- Houses with different siding materials often received two different paint colors. These houses usually have natural colors such as earth-browns, moss greens, sand yellows, and terra cotta reds. The body of the house is often unpainted with stained shingles in brown or red.

These colors were less saturated and earthier than Victorian-era colors.

- While trim colors were used to bring out architectural details, they were chosen to complement the overall color scheme rather than to emphasize specific architectural elements. Trim colors were often white, light yellow, gray and light green. Doors were left unpainted and were varnished.
- Window colors are normally different than the main body and the trim.

#### **8. Exterior Decoration**

- Great degree of ornamentation, but the ornament was used to emphasize the structure and construction of the building rather than to adorn for the sake of adornment.
- Carved rafter ends.
- Decorative gable end trim; knee braces; battered porch piers.
- Window surrounds.
- Column base and capitals.
- Lattice attic vents in the gable ends.

#### **9. Fences**

- The use of fences is not very common.
- A combination of masonry and wood would be acceptable for fences or garden walls, borrowing some of the architectural features used on the main façade.

**X. APPENDIX B**

**List of Acceptable Plants**

The following are considered to be acceptable plants for use in the required streetscape areas (within rights-of-ways). Plants other than these may be considered by the City on an individual basis if they are not on the Prohibited Plant List.

Common Name	Botanical Name	Water Zone
<b>CANOPY STREET TREES</b>		
Bald cypress	Taxodium distichum	M, H
Chinese elm / Allee elm	Ulmus parvifolia 'Allee'	L, M
Eastern red cedar	Juniperus virginiana	L
Florida elm	Ulmus americana var. floridana	M
Laurel oak	Quercus laurifolia	L, M
Live oak / Highrise oak	Quercus virginiana 'Highrise'	L, M
Red maple	Acer rubrum 'Florida Flame'	M, H
Southern magnolia	Magnolia grandiflora	L, M
Sycamore	Platanus occidentalis	M
<b>UNDERSTORY STREET TREES</b>		
Bay tree laurel	Laurus nobilis	L
Chapman oak	Quercus chapmanii	M
Crape myrtle	Lagerstroemia indica	L, M
Dogwood	Cornus florida	M
East Palatka holly (Standard form)	Ilex x attenuata	L, M
Dahoon Holly (Standard form)	Ilex cassine	L, M
Nellie Stevens Holly (Standard form)	Ilex cornuta 'Nellie R. Stevens'	L, M
Magnolia/ Little Gem Magnolia	Magnolia grandiflora 'little gem'	M
Weeping Yaupon Holly	Ilex vomitoria 'Pendula'	L
Fringetree	Chionanthus virginicus	L, M
Taiwan Cherry	Prunus campanulata	M
Italian cypress	Cupressus sempervirens	L, M
Red stopper	Eugenia rhomboda	L, M
Redbud	Cercis canadensis	L, M
Sweet acacia	Acacia farnesiana	L, M
Wax myrtle	Myrica cerifera	L, M
Winged elm	Ulmus alata	L, M
<b>Shrub</b>		
Abelia	Abelia x grandiflora	M
Agapanthus / Nile Lily	Agapanthus	L, M
Adams needle / Beargrass	Yucca filamentosa	L
African Iris	Dietes vegeta	M, H
Australian Jasmine	Jasminum gracile	L
Beach elder	Iva imbricata	L
Blackhaw Viburnum or Rusty blackhaw	Viburnum rufidulum	L, M
Boxwood	Buxus microphylla	M
Burford holly	Ilex cornuta "Burfordi"	M

Common Name	Botanical Name	Water Zone
Butterfly bush	Buddleia asiatica	L
Cardboard plant	Zamia furfuracea	L, M
Christmasberry	Lycium carolinianum	L
Cleyera	Ternstroemia busifolia	L, M
Common buttonbush	Cephalanthus occidentalis	H
Crinum lily	Crinum asiaticum	M
Dwarf yaupon holly	Ilex vomitoria & cultivars	L, M
Flag pawpaw	Asimina obovata	L
Florida anise	Illicium floridanum	M, H
Florida privet	Forestiera segregata	M
Garberia	Garberia heterophylla	L
Glorybower	Clerodendron speciosissimum	L
Holly malpighia	Malpighia coccigera	M
Indian hawthorn	Raphiolepis indica	L, M
Indica azaleas	Rhododendron indica	L, M
Junipers	Juniperus spp.	L, M
Laurustius viburnum	Viburnum tinus	L, M
Leucothoe	Leucothoe axillaris	H
Pineapple guava / Feijoa	Feijoa sellowiana	L, M
Pittosporum	Pittosporum tobira	L,M
Plumbago (Leadwort)	Plumbago auriculata	L, M
Podocarpus	Podocarpus macrophyllus	L, M
Primrose Jasmine	J. mesnyi	L
Rose of Sharon	Hibiscus syriacus	L
Rosemary	Rosmarinus officinalis	L
Rusty lyonia	Lyonia ferruginea	L
Salt bush, (Groundsel Bush)	Baccharis halimifolia	L
Sand holly	Ilex ambigua	L
Sandanka viburnum	Viburnum suspensum	M, H
Saw palmetto	Serenoa repens	L, M, H
Scrub palmetto	Sabal etonia	L,M
Dwarf palmetto	Sabal minor	L, M, H
Schillings holly	Ilex vomitoria	L
Sea lavender	Argusia gnaphalodes	L
Snowberry	Chiococca alba	L, M
Spiraea bridal wreath	Spiraea spp.	L
Swamp honeysuckle azalea	Rhododendron viscosum	H
Knock Out rose	Rosa species 'Knock Out'	M
Sweet viburnum	Viburnum odoratissimum	M, H
Thryallis	Galphimia glauca	L, M
Walter viburnum	Viburnum obovatum	L, M, H
Wild rosemary	Conradina canescens	L
Wild Sage / Lantana	Lantana involucrata	L

Common Name	Botanical Name	Water Zone
Yellow pineland lantana	Lantana depressa	L
Variegated Flax Lily	Dianella species 'Sliver Streak'	L
Palms		
Washington Palm	Washingtonia robusta	L, M
Wild date palm	Phoenix sylvestris	L
Chinese fan palm	Livistonia chinensis	L, M
European fan palm	Hamaerops humilis	L, M
Pindo palm	Butia capitata	L
Needle palm	Rhapidohyllum	M,H
Queen palm	Arecastrum romanzoffianum	L, M
Sabal palm	Sabal palmetto	L, M, H
Medjool date palm	Phoenix dactylifera 'Medjool'	L,M
Windmill palm	Trachycarpus fortunei	L, M
Groundcover		
Asparagus fern	Asparagus spp.	L, M
Bulbine	Bulbine fruticosa 'Hallmark'	M, H
Beach sunflower	Helianthus debilis	L, M
Bigleaf periwinkle	Vinca major	M, H
Bitter panicgrass	Panicum amarum	L
Black eyed susan	Rudbeckia hirta	M
Blazing star liatris	Liatris tenuifolia	L, M
Blue daze	Evolvulus glomeratus	L
Blue lily of the Nile / Nile lily	Agapanthus africanus	L, M
Blue sage	Salvia azurea	L
Blanket flower	Gaillardia pulchella	L
Dwarf Bougainvillea	Bougainvillea 'Helen Johnson'	L
Coontie	Zamia floridana	L, M
Daylily	Hemerocallis spp.	L, M
English ivy	Hedera canariensis	L, M
False heather	Cuphea hyssopifolia and cultivars	L
Holly fern	Cyrtomium falcatum	M
Juniper	Juniperus spp.	L
Lily turf	Liriope muscari 'Emerald Goddess'	L
Mondo grass	Ophiopogon japonicus	L, M
Moss pink	Phlox subulata	L, M
Moss verbena	Verbena tenneusecta	L, M
Parrot plant	Alternanthera amoena	L
Piriqueta	Piriqueta caroliniana	L
Porcupine Grass	Miscanthus sinensis	L, M
Purple heart	Setcreasea pallida	L
Purple queen	Tradescantia pallida	L, M
Rosemary	Ceratiola ericoides	L, M

Common Name	Botanical Name	Water Zone
Sedum	Sedum spp.	L, M
Society garlic	Tulbaghia violacea	L, M
Weeping lantana	Lantana montevidensis	L, M
Wintercreeper	Euonymus fortunei 'coloratus'	M
Wiregrass	Aristida stricta	L
Vines		
Bougainvillea	Bougainvillea spectabilis	L
Cape honeysuckle	Tecomaria capensis	L
Confederate Jasmine	Trachelospernam Asiatium	
Passion flower	Passiflora suberosa	L, M
Crossvine	Bignonia carpeolata	L, M
Downy jasmine	Jasminum multiflorum	M
English ivy	Hedera helix	M
Flame vine	Pyrostegia venusta	L
Florida bonamia	Bonamia grandiflora	L
Running oak	Quercus pumila	L
Trumpet vine	Campsis radicans	L, M
Yellow jessamine	Gelsemium sempervirens	L, M
Grasses		
Bahia grass		L, M
Bermuda grass		L
Centipede grass		L, M
Seashore Paspalum		L
St. Augustine grass		M, H
Zoysia grass		L, M

Note: The use of St. Augustine grass will be limited on streetscapes due to its higher water requirements.

**XI. APPENDIX C**

**Undesirable (Prohibited) Trees**

The following are prohibited to be planted:

Common Name	Botanical Name
Australian pine	(Casuarina spp.)
Bradford Pear	(Pyrus calleryana 'Bradford')
Cajeput or Punk tree	(Melaleuca quinquenervia)
Chinaberry	(Melia azedarch)
Ear tree	(Enterlobium cyclocarpum)
Eucalyptus	(Eucalyptus spp.)
Florida holly or Brazilian pepper	(Schinus terebinthifolius)
Paper mulberry	(Broussonetia papyrifera)
Silk oak	(Grevillea robusta)
Jacaranda	(Jacaranda acutifolia)
Chinese tallow tree	(Sapium sebiferum)
Mimosa	(Albizzia julibrissin)
Poisonwood, Florida poison tree	(Metopium toxiferum)